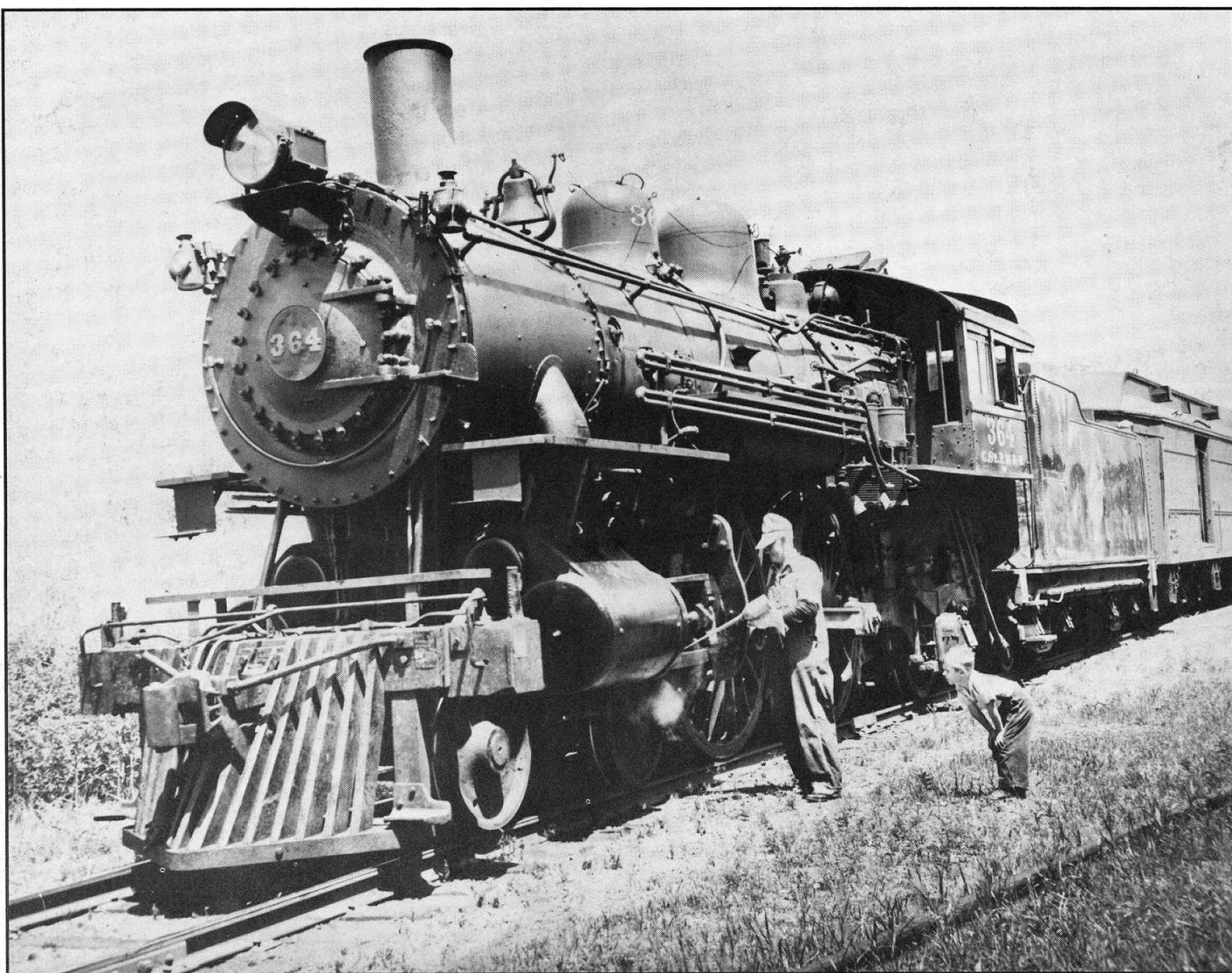


Minnesota Transportation Museum, Inc.

MINNEGAZETTE

Fall 1991







Official Publication of the MINNESOTA TRANSPORTATION MUSEUM, INC.

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CIRCULATION

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SUBMISSIONS

The *Minnegazette* welcomes submissions for publication of articles, photos and other illustrative materials of historical or current interest relating to transportation in the Upper Midwest. No payment is made, and publication is at the editor's discretion.

MUSEUM PURPOSE

The Museum is a non-profit educational corporation organized in Minnesota in 1962 to acquire, preserve, restore and operate historic Minnesota transportation artifacts. It operates the Como-Harriet Streetcar Line and the Minnehaha Depot in Minneapolis, the Jackson Street Roundhouse in St. Paul, the Stillwater & St. Paul Railroad in Stillwater and the Steamboat Minnehaha restoration in Excelsior.

MEMBERSHIPS

Individual	\$ 25	Family	\$ 30
Sustaining	\$ 50	Sponsor	\$ 100

Public Information
(612) 228-0263

Member Information Line
(612) 228-9412

MUSEUM OFFICERS

Chairman	John Diers
Vice-Chairman	Art Nettis
Secretary	Aaron Isaacs
Treasurer	Louis Hoffman
Director	Tom Mega
Directors at Large	Art Pew
	Ross Hammond
	Tom Kolar

TREASURE HUNT

We'd like to hear from anyone who can donating any of the following:

- A small tractor with a backhoe or front bucket.
 - Tuneups for MTM's various automotive equipment.
 - Ballast, railroad ties and used rail.
 - Any streetcar boat hardware or furnishings.
 - Tuckpointing for the Hwy. 95 bridge in Stillwater.
 - A set of crossing signals for McKusick Road.
 - Tarps or plastic sheeting to cover the rolling stock at the arsenal.
 - A laser printer for the Editor.
 - Sandblasting of railroad equipment.
 - Engine oil, batteries and coolant for our diesels.
 - Herbicides for Stillwater weed control.
 - Track machines, especially a tie puller and a new tamper, or repairs to the existing tamper.
 - Free or reduced-price offset printing of tickets, flyers, notices and brochures.
 - Sheet metal and plywood for the PCC restoration.
 - Plants and shrubs to landscape the Linden Hills depot.
 - A display counter for the Linden Hills depot.
 - High quality black and white photo reproduction for the *Minnegazette*.
 - Old railroad books, magazines and collectables for sale at railroad flea markets.
 - Fire extinguishers for the Minnehaha depot.
- Contact the Editor or any Board member if you can help.

THANK YOU FOR YOUR GENEROSITY

To the following persons who donated to the "Save Stillwater" Fund: James Weist, Gordon Forbes, Win L. and Lorraine Brown, Ward Gilkerson and Frank Fust.

To the following persons who contributed to the PCC Fund: Fred Beamish, Douglas Beedon and Charles McCarthy.

To Janet and William Withiam, for their cash donation in honor of Janet's father, Elmer Meyer.

To the following persons who "voted" for their favorite color scheme by donating to the 45-tonner repaint fund: Frank Sandberg, Steve Sandberg and Dick Fish.

To the Carver County Historical Society for donating two TCRT streetcar postcards from the early 1900's.

MEMBERSHIP MEETING NOTICE

The next membership meeting will be held at Prudential on Tuesday, October 29 at 7:30 PM. Prudential is located just south of Highway 12. Exit at Penn Avenue.

CORRECTIONS AND NEW INFO

John Diers believes that the streetcar boat picture on page 18 of the Summer 1991 *Minnegazette* was taken at Wayzata. This would account for the heavy passenger activity.

The Ah-gwah-ching depot shown on page 20 was later renamed State Sanatorium Spur, according to Hudson Leighton. He has compiled a data base of every railroad place name in Minnesota and is adding descriptive information as it becomes available. He is especially interested in the status of depots at the sites, or obscure place names that he hasn't heard before.

Also, thanks to Hudson for solving the mystery of the Markville depot (see page 29).

Tom Lamphier writes to correct the caption about the Great Northern mallet on page 21 of the Summer *Minnegazette*. He writes, "The photo could not have been taken in 1905. The 1254 was originally built by Baldwin in 1907 as an F-8 Consolidation. It was rebuilt in 1910 by the Dale Street Shops into a M-2 2-6-8-0 mallet with the six wheel low pressure engine being furnished by Baldwin.

Front Cover: A picture Norman Rockwell could paint—a small boy watches the ritual of oiling around Omaha 4-4-2 #364 during the Minnesota Railfan Association's first fantrip in 1949. For the whole story, see page 14.

Minneapolis Star-Tribune photo, Minneapolis Public Library collection.

Inside Front Cover: A Duluth Street Railway gate car rumbles across the Interstate Bridge toward Superior, WI circa 1905. Minnesota Historical Society collection.

The rebuilt engine was completed by Dale Street on Jan. 15, 1910. It was renumbered to 2000 on Jan. 18, 1910. It was again renumbered to 1999 on Apr. 12, 1912. In 1926, it was again rebuilt into a C-4 0-8-0 and renumbered to 870.

So the photo on page 21 had to be taken between Jan. 15 and 18 in 1910. Source is R&LHS Railroad History Bulletin 143."

The Watertown, SD depot shown on Page 23 was shared with the Rock Island until its abandonment. Note the outline of the Rock Island shield to the right of the station sign.

CHAIRMAN'S COLUMN

-John Diers



John Diers photo.

This issue of the Minnegazette marks the conclusion of a successful summer.

At Lake Harriet, car 78 took its first bow. Car 1300 emerged fresh from a successful shopping with a new paint job, and hundreds of new visitors brought ridership records to the Como Harriet Line.

At Minnehaha Park, Corbin Kidder and Herb Pinske worked with the Minnesota Historical Society to help the Princess get a much needed rehabilitation.

At Excelsior, Leo Meloche and his crew, thanks to some very

successful fund raising efforts, moved the Minnehaha closer to a re-launching.

Elsewhere, despite ongoing woes with Washington County, a damp spring, and construction chaos in downtown Stillwater, the Stillwater & St. Paul prospered both in ridership and member participation. Thanks to Nick Modders, Art Nettis, Mort Jorgensen, Tom Dethmers, Steve Sandberg, Ward Gilkerson and others unnamed, but not un-appreciated, the railroad carried on.

Finally, and as an altogether fitting finale for a great season, #328 will lead the Victorian Express from Winona to Rochester.

The coming of fall means we turn from running trains and trolleys to fixing them. There will be plenty to do and help is needed. Our passenger cars are looking very tired and Jackson Street will be busy this winter sprucing things up. New projects are also in the offing. Bill Graham needs volunteers to help with the Gopher. In late October or early November PCC #322 will move from Jackson Street to the MTC's shops where an 18 month restoration program will begin. The Board will also have a busy fall and winter. Some tough decisions lie ahead, particularly as the Museum looks to the future and especially as it considers the fate and future of Stillwater.

Stillwater

The Washington County Board adopted an ordinance on August 20th which will kill the Stillwater & St. Paul Railroad and the Zephyr. Its provisions are punishing and vindictive. Faced with the unfounded complaints of a few selfish people, the county showed no courage. A half dozen property owners were able to convince the solons of Washington County that the fleeting passage of a few trains caused unspecified mayhem of the worst sort.

We can ask ourselves, how did this happen? It happened because this bunch could stand up in public meetings and lie. No one on the county board, no one on the county staff or in the county attorney's office subjected any of their wild claims and accusations to cross examination. Never once in all the meetings and hearings were these people challenged

by a board member or a county staffer. The right questions were never asked and truthful answers were never given. Sitting through the meetings was surreal. The same people would get up and tell the same stories. Never mind that we refuted what they said at a previous meeting or that their claims to anyone with any knowledge of railroad operations made no sense at all.

The county staff and the county attorney's office tried to dispose of the whole controversy by drafting an ordinance which split the difference. Unfortunately, what they created was badly flawed and got worse as it passed through the planning commission and reached the county board.

Some people did their homework. Commissioners Don Scheel and Sally Evert came to the issue with open minds. They offered compromises and tried to work with all involved. But they were only two. The others were either hostile or didn't seem to grasp the impact of their actions on the Museum and the Zephyr.

Attempts were made to reach them. Letters were written. Testimony was given by Washington County residents who supported the trains. The Stillwater Chamber of Commerce and the Stillwater business community came out on our behalf, as did the local press in its editorial pages.

Nothing seemed to work. The vote was taken and we lost, but so did Washington County.

In its final form the ordinance restricts the Museum and the Zephyr to 9 trips per week except during September and October when 13 trips are allowed. It requires a conditional use permit and an annual license. It also requires the Museum abandon its storage of passenger cars at Duluth Junction. Other provisions regulate hours of operation, maintenance, and crew qualifications. The ordinance will be effective in November of this year.

The railroad cannot comply with this ordinance and stay in business. The alternatives are to challenge the ordinance in court or shut down.

There are important issues involved which could set a precedent

for other tourist railroads across the country: For example, can, and to what extent, does a local jurisdiction have regulatory authority over a railroad such as ours?

The answers are unknown. Perhaps there are parallels between our situation and the Napa Valley Wine Train. However, to find the answers in court or before the ICC may take years and require great expense. One thing is certain, we will not be forced out of Stillwater and leave behind an investment which was made in good faith with the support and encouragement of Washington County officials.

This may be a long, cold winter.

BOARD OF DIRECTORS

Note: The Summer Minnegazette failed to report that **Barb Paul** was approved as the new MTM Membership Secretary at the June 1991 Board meeting.

July 1991

-Discussed MTM options in response to the Washington County ordinance.

-Directed the General Supt. for Railroad to curtail all non-essential railroad expenses until the Washington County situation is resolved.

August 1991

-Adopted an amended Traction budget that extends through May 1992.

-Approved Louis Hoffman as the chair of the Elections Committee.

ELECTION OF MTM BOARD

Two of the five seats on the Board of Directors are up for election. Under a bylaws change enacted in 1990, Board members serve overlapping two-year terms. The seats currently held by John Diers and Tom Mega will be filled this year. The other three Board members serve one more year.

Ballots were mailed to all members in late September. The

return ballots must be postmarked no later than Oct. 19. The results will be announced at the October 29 membership meeting.

TRACTION REPORT

-Louis Hoffman

Thank You to 1991 Volunteers

No matter what the final ridership statistics show, 1991 will be remembered as a banner year for Como-Harriet. DSR #78 returned to service after a 6 year, \$40,000 restoration. It has performed flawlessly every Monday, Tuesday and Wednesday. The Linden Hills depot opened, marking the end of another multi-year effort. The depot has been well received by our passengers and the expanded line of souvenir items has brought a much needed boost in revenue.

A "tip of the motorman's hat" goes to all of you who have volunteered to make 1991 another successful season: to the foremen and operators, station agents, track, wire, cleaning, grounds and maintenance crews and crew callers. We couldn't have done it without you.

Ridership Remains Strong

On the heels of the best May in ten years (and the fifth best in 20 years), June ridership set a record of 13,696, topping the 13,015 carried in 1972 (when cars ran only from 42nd Street to the car barn). 13,182 were carried on scheduled service, while 514 rode charters. This is up from 10,309 in June 1990.

July ridership, despite poor weather toward the end of the month, remained strong at 14,044 passengers (13,030 regular service, 1058 charters). This was the fourth best July and the best since 1980. August was the best in CHSL history, with 13,326 carried. Through August 31, ridership was 8000 ahead of 1990—on the way to a 50,000 passenger year.

This year, for the first time, weeknight service extended into September. #78 ran the week of Labor Day as an experiment.

In the Media Spotlight Again

The August issue of *Lake Area News* contained an article about #78 featuring



The versatile Burt Foster behind the wheel. Burt knows buses, Burt knows trolleys, Burt knows trains, etc. Louis Hoffman photo.

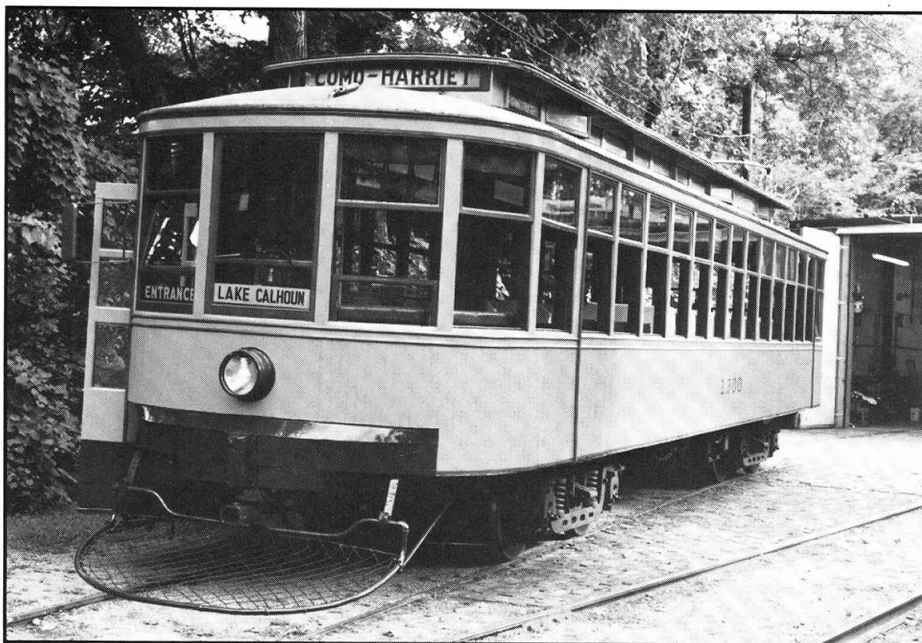
before and after pictures. Earlier in the year, there was a feature on the front page of the *Star Tribune's Variety section*, complete with a color photo of #78 and Roy Harvey. We appeared on KSTP's Good Company shortly before Father's Day. The segment showed Sharon Anderson and son David at CHSL. Our new T-shirt was presented to David on the air. The Anderson/Edelman family, frequent Como-Harriet riders, are now MTM members. Welcome.

Our line has also been featured in the "Traction Currents" section of Railfan & Railroad and the "Rail News" section of Rail Classics. Look for upcoming articles in *Electroliners* and *Locomotive & Railway Preservation*.

#1300 Back from the Shop

The reason #1300 didn't make an appearance until August 1 was a "mini" restoration. It took its turn in the maintenance barn following the completion of #78 this spring. Rotten wood was replaced, seats repaired and painted and it received a new coat of exterior paint. There were numerous other small repairs. The work isn't done. #1300 will spend another winter or two in the maintenance barn.

Late note: a traction motor



Car #1300 looks shiny and new, following its recent repaint. Louis Hoffman photo.

problem has sidelined #1300 for the rest of the season.

This winter, a system of scheduled maintenance for all three cars will be established to keep them all in top condition. Scheduled maintenance will prevent problems from becoming major (and expensive). There will be detailed records kept, ending reliance on memory as to what was done and when.

T-Shirts Selling Like Hotcakes

Through late July, over 225 t-shirts have been sold at the depot. So far, silver is more popular than white and children's size sales are keeping pace with adults'.

If you can't make it to the depot to buy your shirt (and we know you all want at least one), mail orders are being accepted. Specify size (Adult XL, L, M and S; Child L, M and S.) in silver or white. Enclose \$10.50 per adult shirt and \$9.50 per children's shirt. Mail checks to: MTM, P.O.Box 1796, Pioneer Station, St. Paul, MN 55101-1796.

Sweatshirts Also Available

A limited run of sweatshirts featuring the same design as the popular T-shirt is now available. You can buy yours at the annual meeting or by mail (see above). The price is \$16.50 for adults and \$15.50 for children's. Add \$2.00 to mail orders for handling.

1991 Picnic a Success

The 11th Annual Streetcar & Railroad Company Picnic was held at the Como-Harriet car barn on August 18. More than 200 members and their families attended on an autumn-like day. The date was chosen to coincide with the 20th anniversary of the first CHSL service. A banner on the side of #1300 commemorated the event.

Thanks to **Tim Anderson** who did a superb job of coordinating. Thanks



Here's another obscure "first"—the first 3-car lineup at Como-Harriet. The occasion is the Streetcar & Railroad Company Picnic. John Prestholdt photo.

also to the following volunteers who helped set up, cook and clean up:

Hilmer Wagner, Lyndon Benson, Ron Schloer, Harold Dalland, Corbin Kidder, John Dillery, John Maciaczek, Shirley Braun, Gary and Sandy Foss, Mark Jarboe, Paul Joyce, John Skooglun, John Kennedy, Roy and Patricia Harvey, George and Florence Isaacs, Karl and Mary Jones and Mike Buck. Also, a special thanks to **Brian Krynski** and his fellow musicians for the fine music from the rear platform of car #265.

Linden Hills Appreciation Days

For the third year, CHSL will hold its Linden Hills Appreciation Days. This year's event will be October 26 and 27. The fare will be 25 cents as our way of thanking the community for its support.

Halloween on the Streetcar

The last day of operation this year will be Halloween Day, Wednesday October 31, from 4:30 to 6:30 PM. All children in costume will ride free.

PCC Restoration to Begin Soon

If all goes as planned, work will begin on PCC car #322 sometime in November. It will be trucked to the MTC's Overhaul Base on Cleveland Avenue in St. Paul, across the street

from the Minnesota Commercial roundhouse. Museum crews are assembling a special track section to hold the car, and making a trip to Gomaco in Ida Grove, IA for windows and window mechanisms.

If you are interested in any part of the restoration, call **Louis Hoffman** at 296-9418, **George Isaacs** at 484-7512 or **John Diers** at 929-5699.

More Volunteers Needed for '92

To keep up the pace of the 1991 season, the Traction Division will need:

- More operators willing to work weekends.
- More station agents.
- More car cleaners.
- More groundskeepers, and someone to organize and schedule them.
- More charter crews.

During this year, a very small number of people worked an awful lot of weekends. If everyone who doesn't normally work weekends did so just once, the problem would be solved. The alternative is to cut back weekend service—when we carry most of our passengers. That would be a financial disaster.

Likewise, if each operator volunteered just once to staff the depot, we'd have no problem having it open all weekend and some weeknights as well.

Our cars are never truly dirty, but



#78 looks good from the top. Louis Hoffman photo.

they should be cleaned more than once a month. That is all we are able to do with the present small but dedicated band of volunteers.

As many of you know, grounds-keeping, which involves mowing and trimming one mile of right of way, is done by **Mike Buck**, **Roy Harvey** and a few others. If we can't get more help, especially an organizer/scheduler, we will have to hire a contractor, at significant cost.

Several charters have been canceled for lack of crews. If you are retired,

unemployed, have time off during the day, or can take time off from work, please call **Jean Villardson** and let her know you're available. Charters are a good way to increase revenue. Many involve school groups, and our mission is to educate the public. We don't actively promote charters, but if we had more crews we could, and there's a huge market waiting to be tapped.

We appreciate all our volunteers, no matter what their level of involvement. Please consider giving the museum just a little more of your time. When the Traction survey arrives early next year, please keep this in mind.

RAILROAD REPORT

Equipment

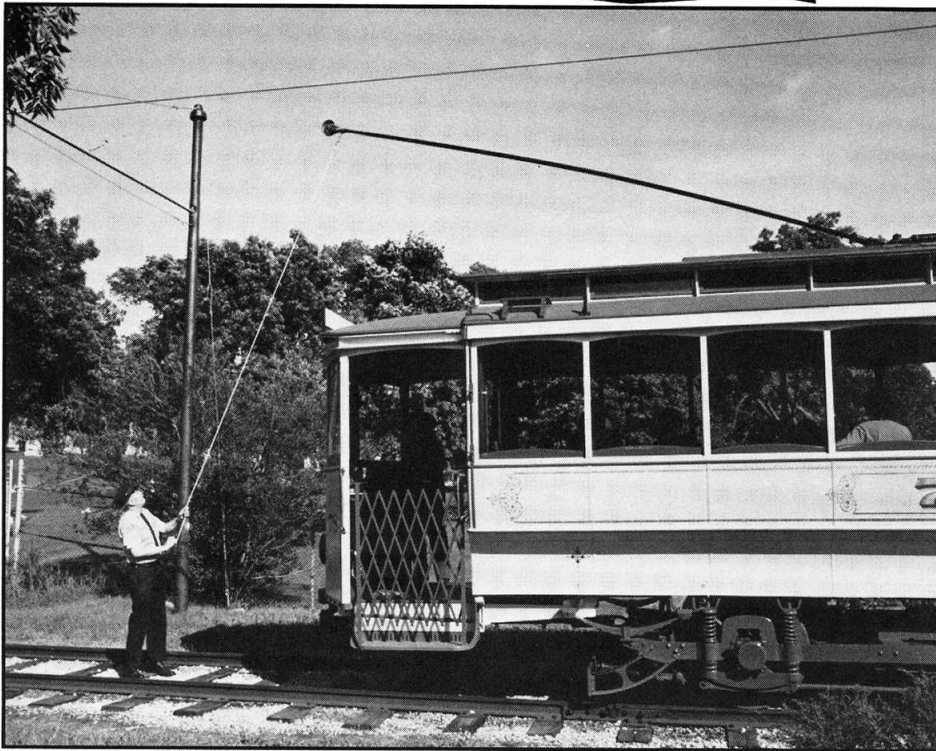
Steve Sandberg notes that MTM had all seven of its coaches running on the day of the Andersen Window picnic. That is three months earlier than last year. Generally, he says, the equipment is in better shape this year than last.

C&NW/GN coach #1096 is back in service. It received new window seals all around and air brake work. Several weak spots in the floor were replaced.

GN coach #1213 has all new brake gaskets and hoses. It is in the best condition of all the lightweights.



Brian Krynski's quintet entertained the annual picnic with medieval music from the back platform of car #265. John Prestholdt photo.



One of car #78's subtle pleasures is walking the pole around at each end of the line. Corbin Kidder does the honors at Lake Calhoun. Note the smaller diameter Duluth trolley wheel. John Prestholdt photo.

Up at the arsenal, crews have put a tarp over NP heavyweight coach #1370. Its roof leaks badly and it has sustained much water damage inside. NP lightweight coach #598 was partially tarped to keep water out of a couple of broken windows.

By the time you read this, GN heavyweight baggage car #480 may

be getting fixed up for service, possibly on the Victorian Express. It needs sandblasting, painting and the installation of car-end diaphragms. If it gets a repaint, the Westinghouse boxcab diesel will as well.

An investor group called Northern Railtours is interested in restoring one or more mainline steam engines for

excursion service. They have inspected NP 4-6-2 #2156. If they like what they see, they may enter into some kind of agreement with MTM.

Ridership Down

As of August 31, ridership was lagging noticeably behind 1990, but slightly ahead of 1989, although charters are up. Rainy weather has been the main culprit but the extensive street and utility work in downtown Stillwater has not helped matters.

Reorganization

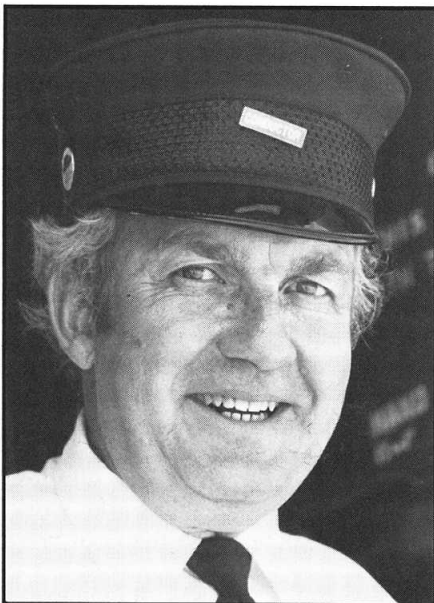
There have been a couple of changes in the management structure of the S&StP. Art Nettis has moved from Trainmaster to Assistant to the General Superintendent. The new Trainmaster is **Mort Jorgensen**. Larry Schulte has replaced Scott Heiderich as Diesel Foreman. Crew calling for train crews, coach attendants and ticket agents has been consolidated. In the past each had been done separately. **Mort Jorgensen, Burt Foster and Marv Mahre** will alternate in the job.

Maintenance of way

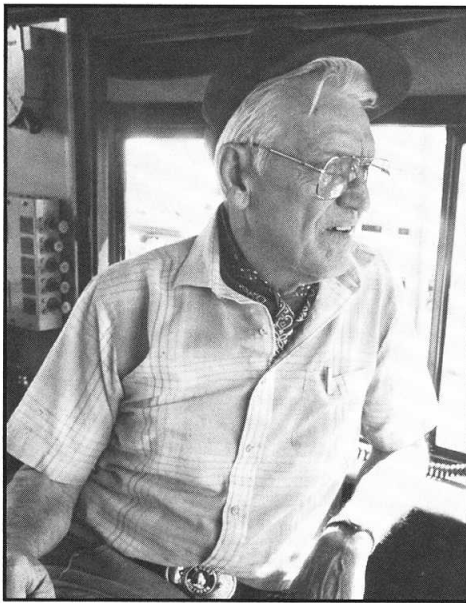
Chief Engineer **Bill Marshall** has been busy. He is writing a maintenance and trouble shooting manual for the Manning Avenue crossing signals. **John Stein** is helping with the layout and printing. When it is completed, Bill will teach a class in signal maintenance and hopefully recruit some volunteers for that job.

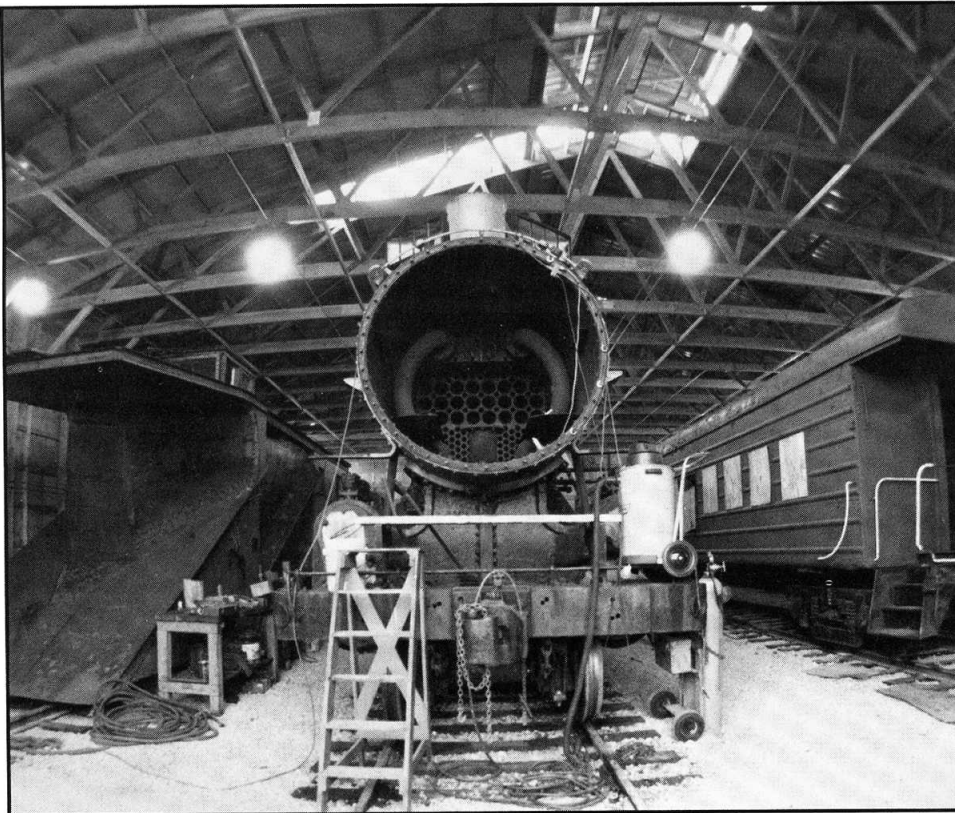
Bill is working with the county to help plan the widening and curbing of McKusick Road, so that the completed project will be compatible with MTM's needs. This includes improved drainage, adequate room for future crossing signals, an electrical conduit under the road and a maintenance access driveway. In addition, he recently resurveyed the curve just west of the crossing and found it to be slightly irregular.

Due to the uncertain future of the S&StP and MTM large legal bills, trackwork has been reduced as much as possible. Only small crucial jobs are being done. For example, curve #1 south of the bridge along



Nick Modders was the conductor and Tony Becker the engineer on a May 1991 Stillwater charter. Cliff Buchan photos courtesy Forest Lake Times.





With its previous occupants now at Stillwater or under cover at Smith Brothers, the NP Russell plow, steamer #2156 and Milwaukee combine #405 get relief from the elements at Jackson Street. Hudson Leighton photo.

Hwy. 95 had gauge that was 1.5 inches wide. It received 110 new ties, plus ballast.

Between Marylane and Manning Avenue, there was very little ballast. The rail was loose and would rock from side to side. There was a large dip in the track. 120 tons of rock was spread, permitting some of the track to be raised 14 inches.

There has been a muddy dip under the golf cart crossing at Oak Glen. By the time you read this it will have been raised, with new ties and ballast.

That leaves 10 mph slow orders just west of the Hazel Street and Sawmill grade crossings. Ex-Union Pacific track engineer **Jim Dunn** has taken another look at the line. He recommends removing the gauge bars, adding anchor bolts on the welded rail and improving drainage along the entire route.

Finding a Home in Stillwater

Now that the City of Stillwater owns part of the BN's yard, changes are already underway. The city's goal is to compress the spread-out yard

into a smaller space, while retaining about the same storage capacity. This will create room for more parking on both the river and town sides of the yard. There are no immediate plans to eliminate the yard altogether, as was reported earlier.

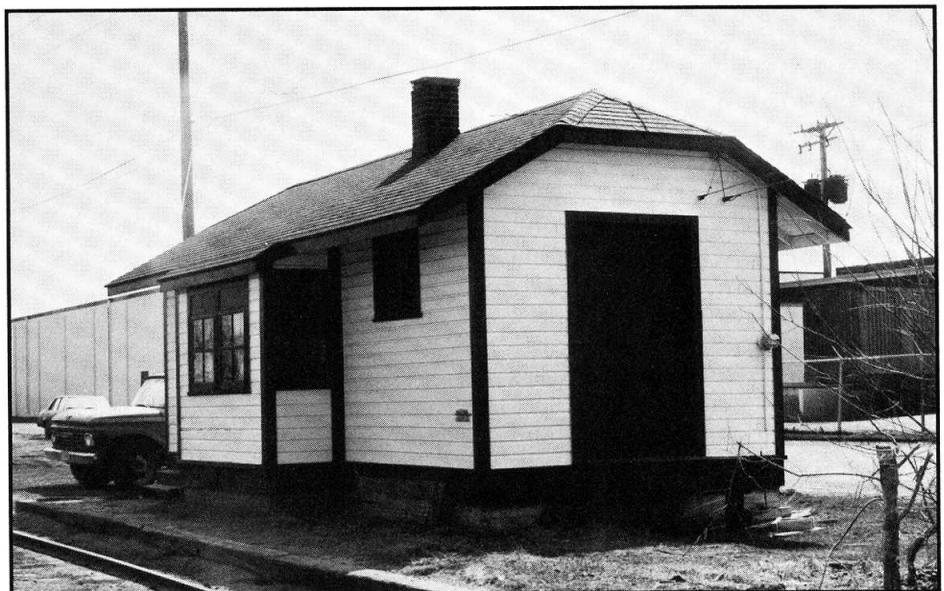
The tracks west of the yard office

have been removed and the "lake spur" closest to the river has been shortened. To make up for the lost capacity, the short track in front of the BN yard office, where the Minnesota Commercial diesel is spotted, is being extended south. A new switch will be cut into the mainline just north of Hooley's parking lot, creating a second runaround track. It now looks like this may become MTM's new loading track.

The museum's Stillwater Depot Committee (**Mark Balay** and **Bob Thompson**) has been meeting with city planner **Steve Russell** to find an alternative to the present loading area. In the process they may have found a home for the former Milwaukee Road Bayport depot, which currently sits up on blocks next to the Zephyr and must be moved soon because of construction. The city may agree to relocate it on city property just south of the yard office. It could then be used by MTM. As this is written, no firm decision has been made.

Washington County

The summer has been a real roller coaster ride, as the Washington County zoning ordinance weaved its way toward adoption. Since the last Minnegazette, the ordinance was recommended by the Planning Advisory Commission and adopted 3-2 by the County Board. This



The Bayport depot in 1985, while still in use. It may find a new home in Stillwater as MTM's depot. MTM collection.

happened despite heated opposition from the City of Stillwater and the Stillwater Chamber of Commerce. In its current form, it restricts the number of weekly trips to nine for the Zephyr and MTM combined, except during September and October when thirteen weekly trips are permitted. Only four Saturday and two Sunday trips are permitted, regardless of the month.

It forces equipment storage out of Duluth Junction after one year and prohibits passengers loading and unloading there. It would require both MTM and the Zephyr to apply for conditional use permits and annual licenses. There are a number of lesser restrictions.

The ordinance was adopted in July, but the County Board forgot to rezone MTM's property. This forced them to reconsider the issue on August 20. John Diers lobbied the board members hard, trying to get one vote changed, since this would have killed the ordinance. The result was a slight loosening of the trip count, but the final vote went against MTM. Our thanks to County Commissioners Sally Evert and Don Scheel for supporting the MTM position.

The ordinance becomes effective November 1991. This means the Zephyr will have to apply at that time for a permit to run in 1992. Should MTM choose to seek a permit, the application would be made in the spring of 1992. Zephyr owner Dave Paradeau has put his depot construction plans on hold indefinitely.

ICC Common Carrier Status

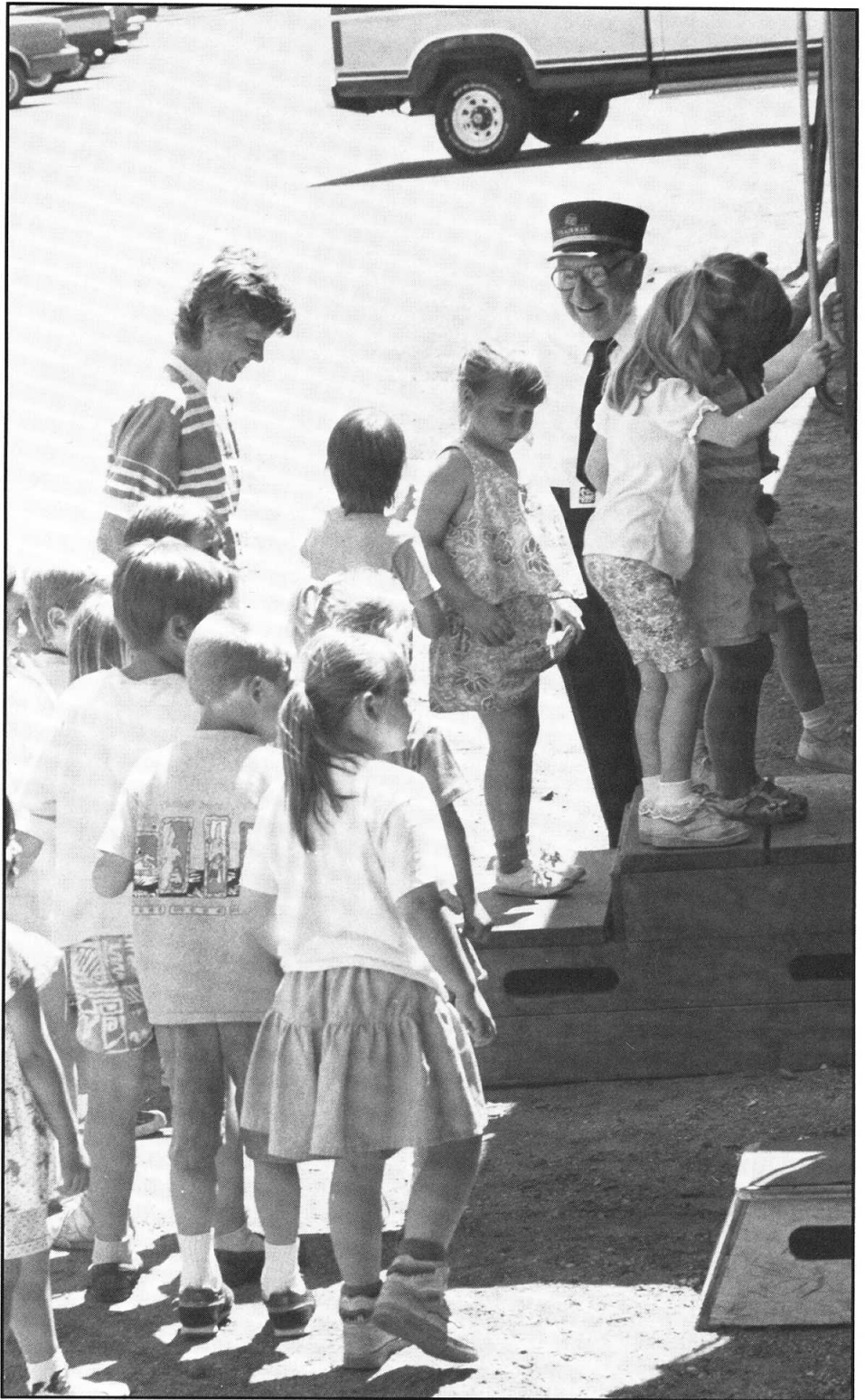
The Stillwater & St. Paul is now an ICC-certified common carrier. Thanks to Barney Olsen for guiding the application through the Washington bureaucracy. In a separate transaction, the Zephyr has also applied to the ICC to become a common carrier and has talked with the Chicago & North Western about running south toward Hudson, WI.

According to a story in the Star-Tribune, the ICC staff believes that the common carrier status applies only to freight and that the passenger service is subject to local control. As this is written, the ICC

How can anyone believe that our train should be banned after looking at these pictures? This charter gave 400 elementary school children their first train ride and they will probably never forget it. Ray Norton helps with the boarding.

Opposite Page : Amy Newmann of Holland School in Minneapolis watches out the baggage door of car #1102.

Cliff Buchan photos courtesy of Forest Lake Times.





itself has not addressed the issue. However, it has rendered an unfavorable opinion in the Napa Valley Wine Train case. The Wine Train runs a Zephyr-like operation on a ICC common carrier railroad, but the ICC has said that the common carrier status does not apply to the Wine Train, and it can be regulated by the California Public Utilities Commission.

Victorian Express

For the second year, MTM was involved with the Victorian Express, which ran Oct. 5 and 6. Sponsored by the Winona County Historical Society, this year's trip travelled between Rochester and Winona via the Dakota, Minnesota & Eastern, formerly the C&NW and originally the Winona & St. Peter. This was the first public passenger train over the route of the Dakota "400" in 28 years and the first steam powered train since the mid-'50's.

MTM's steamer #328 led the train, with a diesel helper. MTM also supplied coaches #1096 and #1213. MTM members served as coach attendants. **Dennis Johnson, Steve Sandberg** and **Nick Modders** were heavily involved in planning and running the trip.

TRAIN Convention Coming

The Tourist Railway Association national convention is being hosted

by the Lake Superior Museum of Transportation in Duluth Oct. 31-Nov. 3. The program will include:

Thursday Oct. 31- An excursion to Virginia via the Duluth, Winnipeg & Pacific and a visit to Ironworld.

Friday Nov. 1- All day seminars and an evening excursion to Two Harbors on the North Shore RR.

Saturday Nov. 2- Seminars all day at the Lake Superior Museum, and an evening banquet. Wisconsin Central President Edward Burkhardt will be the speaker.

Sunday Nov. 3- Board meeting.

It is customary for all nearby TRAIN members to put on a show for the visitors.

MTM is planning a special tour for Wednesday October 30. It will begin 7:30 AM with coffee and rolls at the Jackson Street Roundhouse. At 8:15 GMC bus #1399 will leave for Lake Harriet. After trolley rides and a carbarn tour, the next stop will be the Minnehaha Depot at 11:00. Then on to Stillwater for a special train ride 12:30. After lunch, the bus will return to Jackson Street Roundhouse. Tour tickets are \$33. The price includes a box lunch, the bus and all MTM rides. Members may buy a ticket for \$29, but non-members will get first seating on bus #1399. Send your check to the P.O. Box 1796 address.

Louis Hoffman is coordinating the event.

White Bear Lake Depot Reopens

The Northern Pacific depot in White Bear Lake has been purchased by the city and restored. The agent's office will house the Chamber of Commerce. The White Bear Lake Historical Society is using the baggage room as a display area. The dedication is scheduled for the first week in November. It is possible that MTM will provide an excursion trains to Hugo as part of the festivities.

STEAMBOAT REPORT

Leo Meloche reports that the Wednesday evening and Saturday morning work sessions have been well attended, with 6-10 volunteers on most days. Interestingly, it isn't always the same people working. New faces appear all the time.

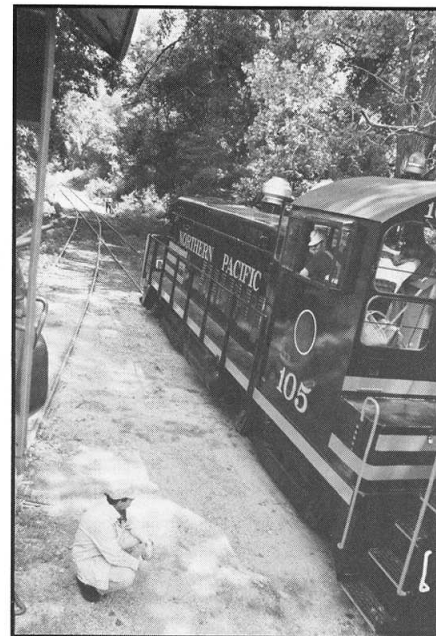
The last Minnegazette reported that 20 of the 138 ribs had been replaced. That number has now passed 70, over halfway there.

The new Navy engine has been delivered. Unexpectedly, **Chadwell O'Conner**, the seller, threw in a panel of steam gauges valued at well over \$1000. By happy coincidence, it resembles the Minnehaha's original panel.

Sales of "The Streetcar Boats" print have been phenomenal. 160 signed artist's proofs have sold for \$100, or \$250 framed. An additional 1800 non-proof prints have been ordered, and about 200 have been sold. Clearly the prints, bought mostly by Lake Minnetonka area residents, have touched some kind of nerve with the public. Credit member **Jim Ogland** for the success of the print sales program.

The boat building was never intended for tours. However, many people have heard about the Minnehaha and want to take a look. Ogland is building a display "depot" underneath the new storage scaffolding in one corner. It will receive a wood floor and will house the wheel and other boat artifacts while they await installation. There will be displays of pictures and souvenirs will be offered for sale.

The boat Harriet, also called the



JID, was profiled in the Summer Minnegazette. It has been purchased by **Fred Brunchion**, who owns the Mai Tai restaurant in Excelsior. He is having the boat restored to its original appearance at the Riverside Yacht Service in Hudson, WI. The previous owner had made various minor modifications, such as a fiberglass roof. When it was installed, it evened out the roofline. Originally, the front quarter of the roof was raised somewhat. The enclosed part of the cabin had been lengthened and will be reopened. There were also some patch-type repairs to the keel that need to be undone. When complete, it will return to Lake Minnetonka.

If you wish to donate to the Steamboat Fund, send your gift to: Minnesota Transportation Museum, c/o Norwest Bank, 411 Water Street, Excelsior, MN 55331.



Always looking for the fresh photo angle, **Benn Coifman** rode the Zephyr's cab. Above Left: Engine #105 with the Pullman green "local" consist passes the Zephyr spur at 12:05 PM. The dinner train will follow it up the hill.

Above Right: Zephyr engineer **Jack Anderson** does the rollby inspection.

Center: The scheduled 2:30PM meet at Summit.

JACKSON STREET REPORT

Minnegazette readers may have noticed that there has been little news from Jackson Street Roundhouse recently. Much has been waiting for the groundwater pollution studies to be completed. A large cleanup could disrupt much of the sight, so all new construction has been put on hold.

Art Pew used to store his private car, the Gritty Palace, on a BN spur

just west of Dale St. in St. Paul. The car is now based in Chicago, but he still pays rent on the spur. He has arranged to have the 600 feet of track removed. The materials will be stored at Jackson Street for reuse in the future. Art is donating the materials to MTM.

A contractor removed a large amount of brush from the Pennsylvania Avenue side of the property in late August. The brush hid anyone who might want to penetrate the fence. It was also a fire hazard. This is part of the continuing effort to clean up the site.

NEW LOOK FOR THE MINNEHAHA DEPOT

Corbin Kidder has been trying to get the Minnehaha Depot repainted for about five years. His persistence has more than paid off. The Minnesota Historical Society (MHS) recently completed a professionally done rehab of the building's exterior.

Corbin had a Boy Scout troop ready to put on a new coat of paint last year. When MHS, which

Looks like a Chicago commuter train, but it isn't. That smoke coming from the wrong end is emitted by C&NW 4-6-0 #1385. Together with "Executive F" #403, it ran in push-pull service during St. James Railroad Days on June 23, 1985. Perry Becker photos.

actually owns the depot, discovered that it had layers of lead-based paint, they decided to strip it down to the wood. It took much of the winter to let bids and hire a contractor.

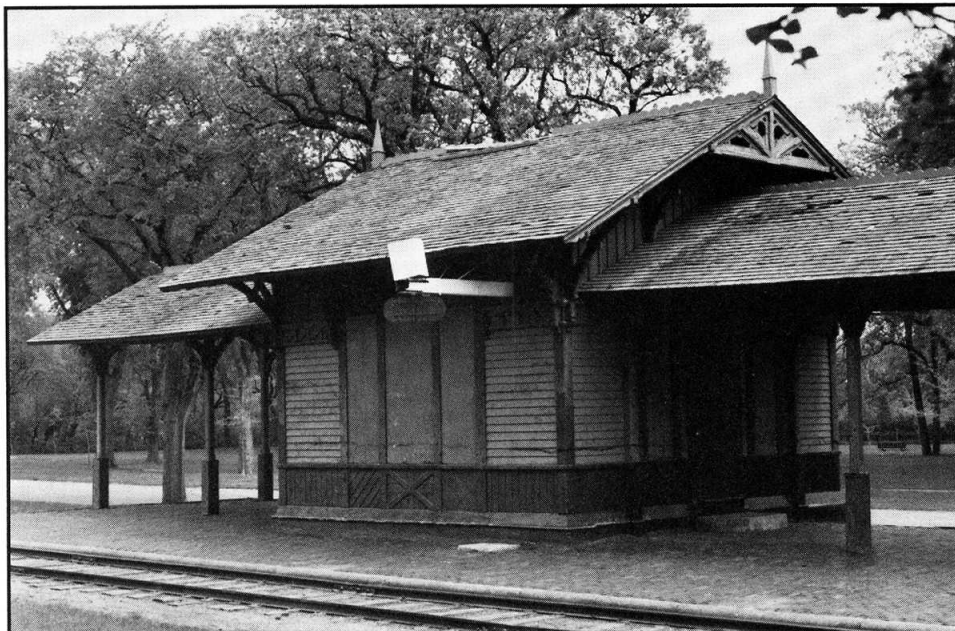
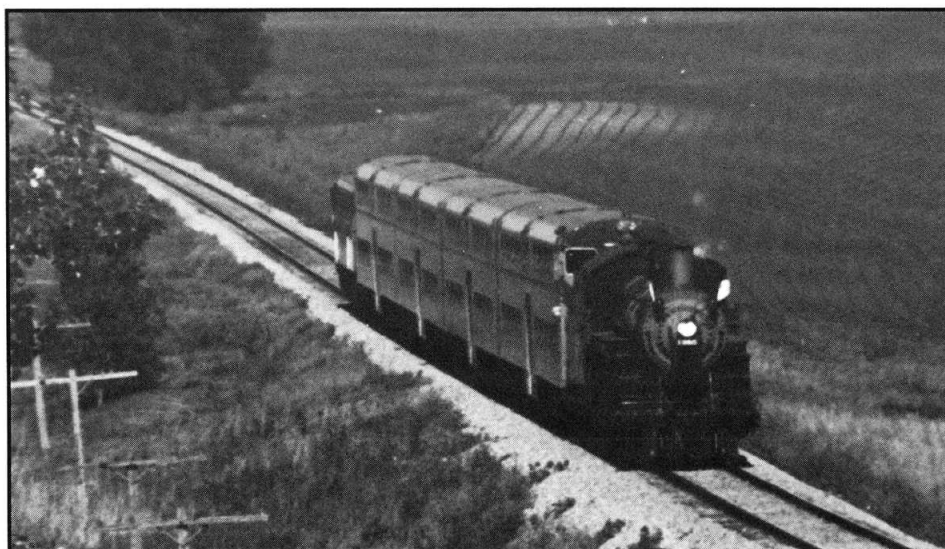
In the process of stripping, they found that the orange and maroon colors were not the originals. The orange was fine, but the maroon has been changed to a chocolate brown.

Rotted wood was replaced around the portico pillars, on the fascia boards, and on the order board. Over the years, some of the platform bricks have been stolen and will need replacement.

When the repairs permitted, the depot was open as usual on Sunday afternoons for the public.

The depot twice played host to visiting railroad equipment this year. A Canadian Pacific business car parked there overnight and plugged into the depots electricity.

For the annual A Fair of the Heart in September, the Soo Line displayed a diesel at the depot.



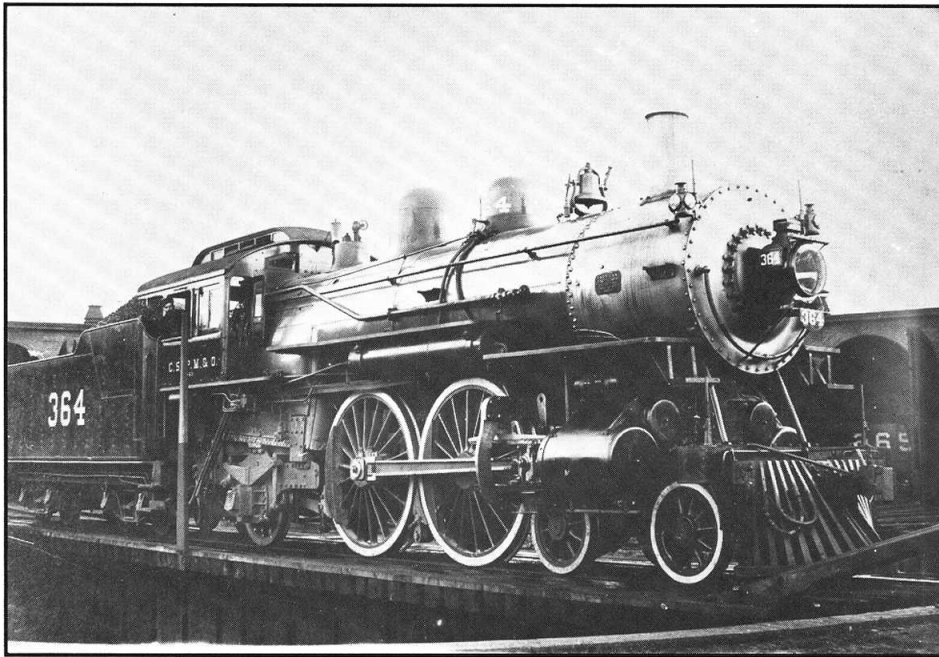
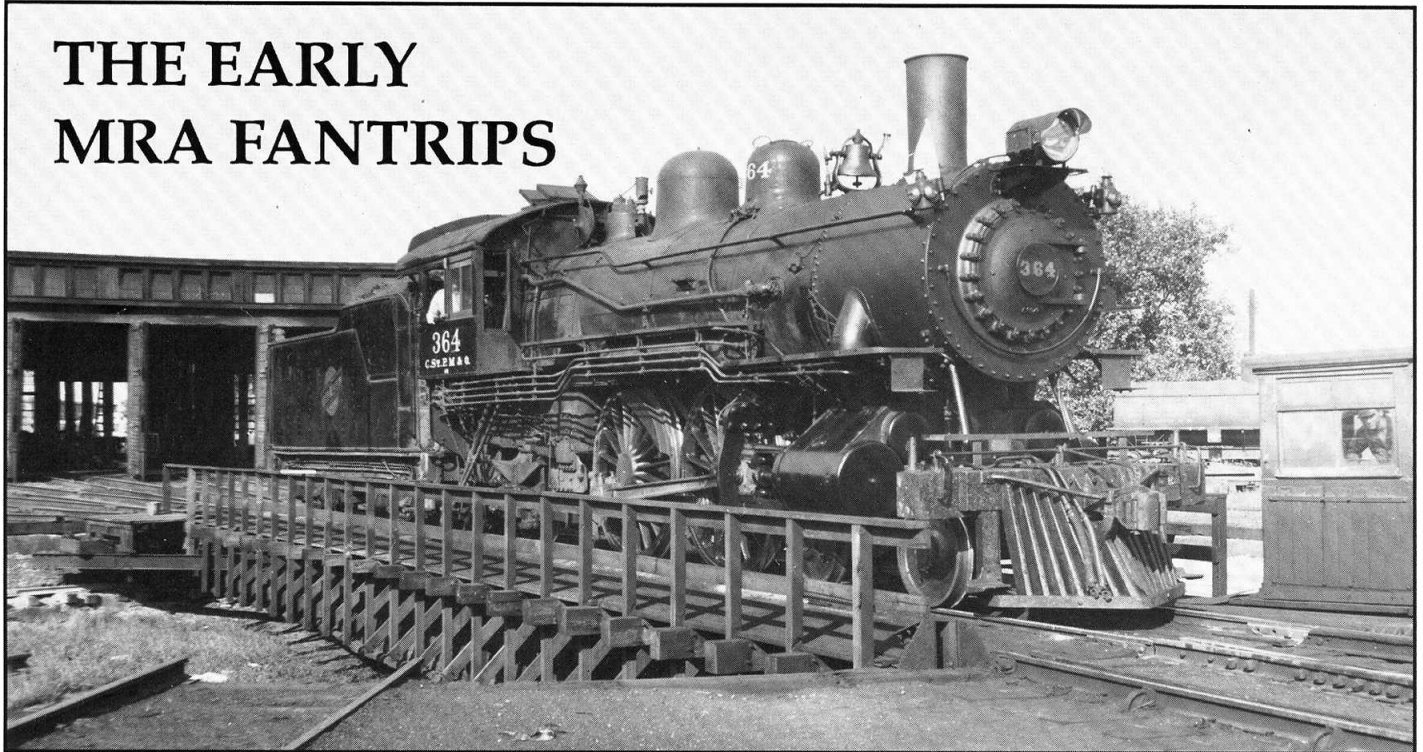
The Minnehaha Depot looked like this until recently. Note the faded paint, rotting order board and fallen gingerbread on the roof. MTM collection.

TCRT VIDEO COMING

A new Twin City streetcar video is in production, with MTM's help. Member Don Olsen of Catenary Productions is projecting delivery in late 1992. Russ Olsen is representing MTM. They have gathered together every known piece of footage. Russ is writing the narrative. Contact him if you have any Twin City or other Minnesota traction movies, or anecdotal materials, so they can be included.

Don has produced excellent videos on the Sacramento Northern, the North Shore Line and the Northwestern Pacific. His current project is on San Francisco Bay ferry boats. If any member is interested in helping design the cover, please contact Don Olsen, Catenary Video Productions, 795 Burnett Ave., #2, San Francisco, CA 94115.

THE EARLY MRA FANTRIPS



Compare these pictures of #364 at the beginning and end of its life. Above: On the turntable at north Minneapolis in 1949. R.V.Nixon photo. Ray Norton collection.

Left: It looked like this at the St. James roundhouse in its early days, with a centered oil headlight, clerestory cab roof, spoked pilot truck wheels and less plumbing. Watonwan County Historical Society collection.

Editors note: Longtime MTM member **Ray Norton** was involved in some of the first fan trips run by the Minnesota Railfans Association over 40 years ago. In this interview, Ray shares his recollections about the trips.

Minnegazette: Can you tell me when the Minnesota Railfans Association started?

Ray Norton: Well, not for sure, but **Charlie Sulzbach** started it and I first became involved in it when Charlie was telling me that the

Milwaukee Road didn't want to talk business with him for a railfan special. I said, "Let's try the Omaha and see what happens." And that's exactly what we did. It was with the Omaha trip that I first really became involved with the MRA.

MG: And what year was that?

RN: About 1948 or 49.

MG: Tell us about the first trip. How did that begin?

RN: Well, we got ahold of the Omaha, and they said if we do any business it will have to be cash on the

barrelhead in advance. And we said we expected to pay for it, but also wanted to specify what we would do and how we would do it. They said, well, if it's within reason we'll do it. We requested the oldest equipment available and some reasonably good equipment plus the old Atlantic that was filling in when the motor car broke down on the Namakegon. They said, well, we think we can arrange that, so they gave us three air conditioned cars and three gas-lit steel cars. They could not give us wood ones. They did not have wood ones left anymore. They brought the 364 in from Spooner, which was its normal layover place to protect the gas car on the Namakegon. The next thing was, where did we want to go, and I had the idea that it would be a lot of fun to travel a circle.

So we left Minneapolis and went



The special poses atop the Plymouth Avenue overpass on its way to pick up the passengers at the GN Depot.

It's a three-way meet at Chestnut Street. #364 and an RS-1 powered freight wait for a Milwaukee passenger to clear the junction. Both R.V.Nixon photo. Ray Norton collection.





Above: Stopping for lunch at Mendota station. Note the Milwaukee Road's line from Minnehaha Park crossing over in the background.

Below: The excursion pauses at Merriam Junction, before returning to Minneapolis via the M&StL and Hopkins.
Both R.V.Nixon photos. Ray Norton collection

to St. Paul, and this is the only time I've ever known a full fledged passenger consist to go through St. Paul Union Depot like it wasn't there. In fact, one of our railfans, **Harold Ashcroft**, was so sure that they'd stop, that when they got down to a slow walk he dropped off and went into the office. He had to take a taxicab to get down to the Omaha's main locomotive shops at Randolph Street, 'cause that's where we went first.

We pulled up from Cliff into the Randolph Shop area. They gave us a guided tour of the shop area. Among other things was the world's largest Pacific being worked on, the 602, and there was a bunch of other equipment around. And then after that we backed down to Chestnut and we rolled over to Mendota and had ourselves a picnic lunch. Then we took off from there and we went through Nicols, Savage, Shakopee, and over to Merriam. In the morning they had a J derail on the short leg of the wye at Merriam, so there was a lot of conversation before they would allow us to run the wye. But they finally decided that the Atlantic was small enough to go where a mike had had trouble.



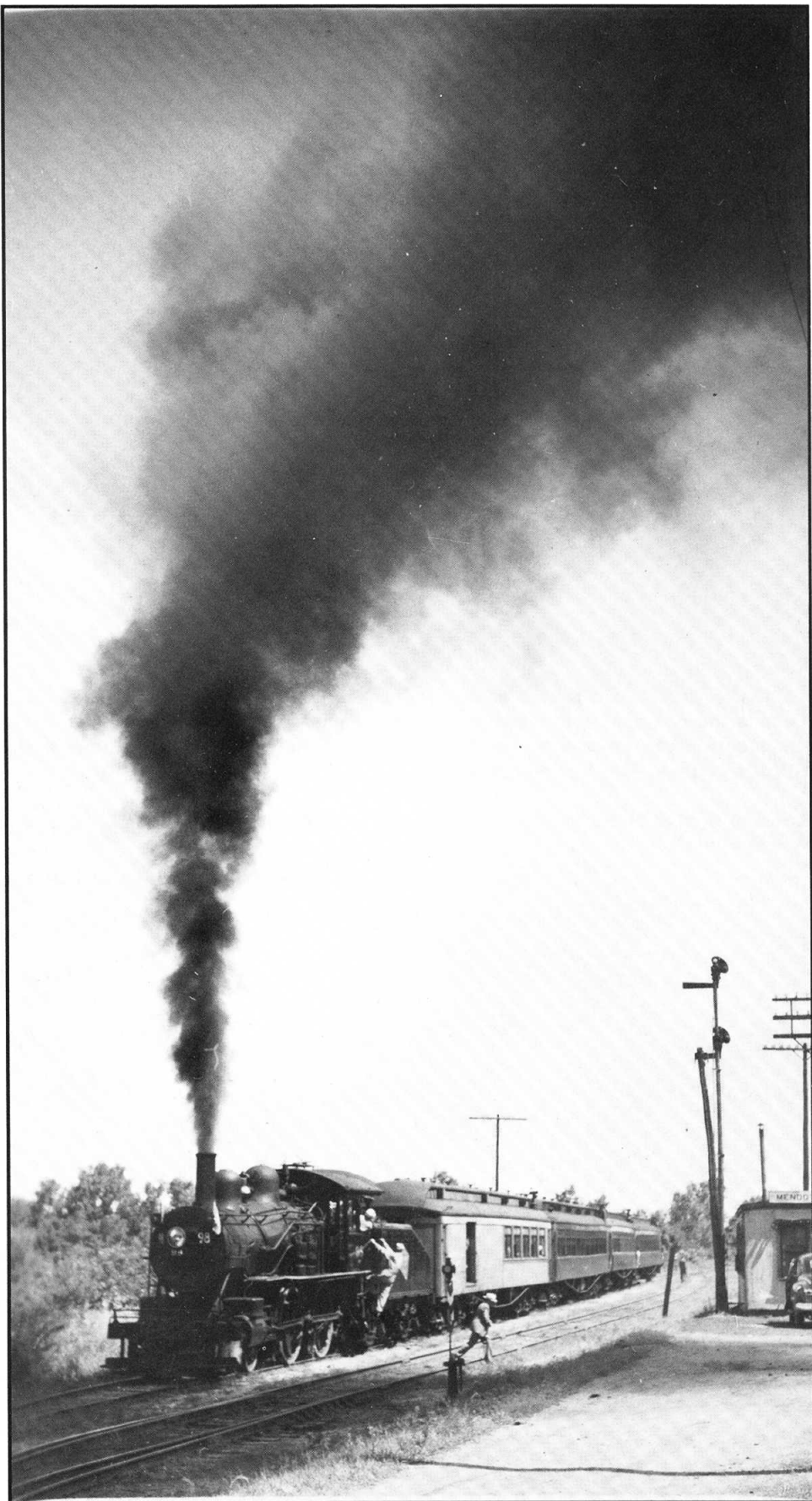
So we pulled around the short leg of the wye and we came back in over the Minneapolis & St. Louis. I was fortunate enough to be able to ride the cab from Merriam to Minneapolis. I was sitting on the fireman's seatbox. He had the opportunity to stand and quite frequently shovel. And the old hoghead, he was a real sharp looking guy, with a wonderful grin. When we went across the Main Street crossing at Chaska, we went rather slowly because the book said so. There was the Milwaukee crossing there. At that time the depot was a Union Depot between the Milwaukee and the M&StL and they handled the train orders for the Omaha, because the Omaha had trackage rights on it for their freight trains. Well, this old hogger, all he did was widen on the throttle but he didn't drop that 84 inch Johnson bar down toward the corner at all. He left it well hooked up. We took off and we hit the bottom of the hill at about 45 miles per hour and the Travelling Engineer said, "By golly I'd have dropped her and really wound her up 'cause we've got a heavy hill ahead of us." The old hogger gently caressed the Johnson bar, never having dropped it a mite and we rolled over the top at about 45 miles an hour. That showed you what experience will do. And if I remember right, we got back into Minneapolis within 15 minutes of our advertised time. That was the way the Minnesota Railfans first railfan trip operated.

MG: What sort of speed did you attain with the 4-4-2?

RN: With that old Atlantic we probably got close to 70 in between Nichols and Savage. He had her wound up real well and those 81-inch drivers didn't have to turn over too often cover a mile.

MG: I noticed in some of your news clippings that the engine had made a record breaking run from Minneapolis to Chicago.

RN: Back in the old days they used to have silk trains. The silk would hit the west coast and they'd want to get it east to the markets as quickly as possible. So on this particular occasion that engine was one of the locomotives that figured on the silk train and it made "400"



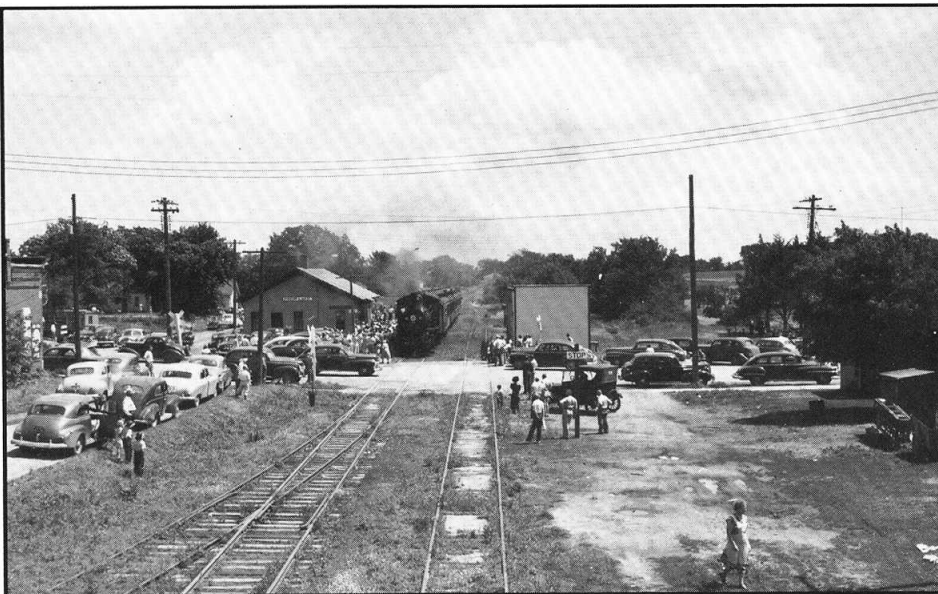
#98 has just run around its train at Mendota, and is about to head south to Farmington. R.V.Nixon photo. Ray Norton collection.



Here is the companion to a picture in the Summer 1990 Minnegazette. The excursion is running tender first across the Minnesota River, taken from the Mendota bridge. Note the West 7th Street bridge and Fort Snelling in the background. R.V.Nixon photo. Ray Norton collection.

Looking right at home among the weeds and light rail, #98 approaches Shakopee. John Malven photo. Ray Norton collection.





Prior Lake turns out to greet the MRA special.
R.V.Nixon photo.Ray Norton collection.

time way back in 1909. That was before they had the railroad completely double tracked or straightened out and they had to stop for water periodically. So that was a truly remarkable run.

MG: The 364 was almost in original condition, wasn't it?

RN: Yeah. About the only thing they had done was superheat it. She

still had her wooden pilot and pilot beam.

MG: How long did the engine last after your fantrip?

RN: You know the tragic thing is that it was given to the MRA for a dollar and we couldn't find a place to put it. So I would guess it lasted as long as the Namakegon.

MG: So it was actually signed over to MRA?

RN: Yes.

MG: Was that the first fan trip that you knew about in the Twin Cities area?

RN: Yes, it was. We'd had fan trips on the streetcars, but not railroad.

MG: How did you personally get involved?

RN: Well, I knew a little bit about who to go through to get these trips arranged-like I went through Steiner, General Passenger Agent for the Omaha in St. Paul to get that one arranged, and he cooperated so beautifully.

MG: What sort of turnout did you have for the Omaha trip?

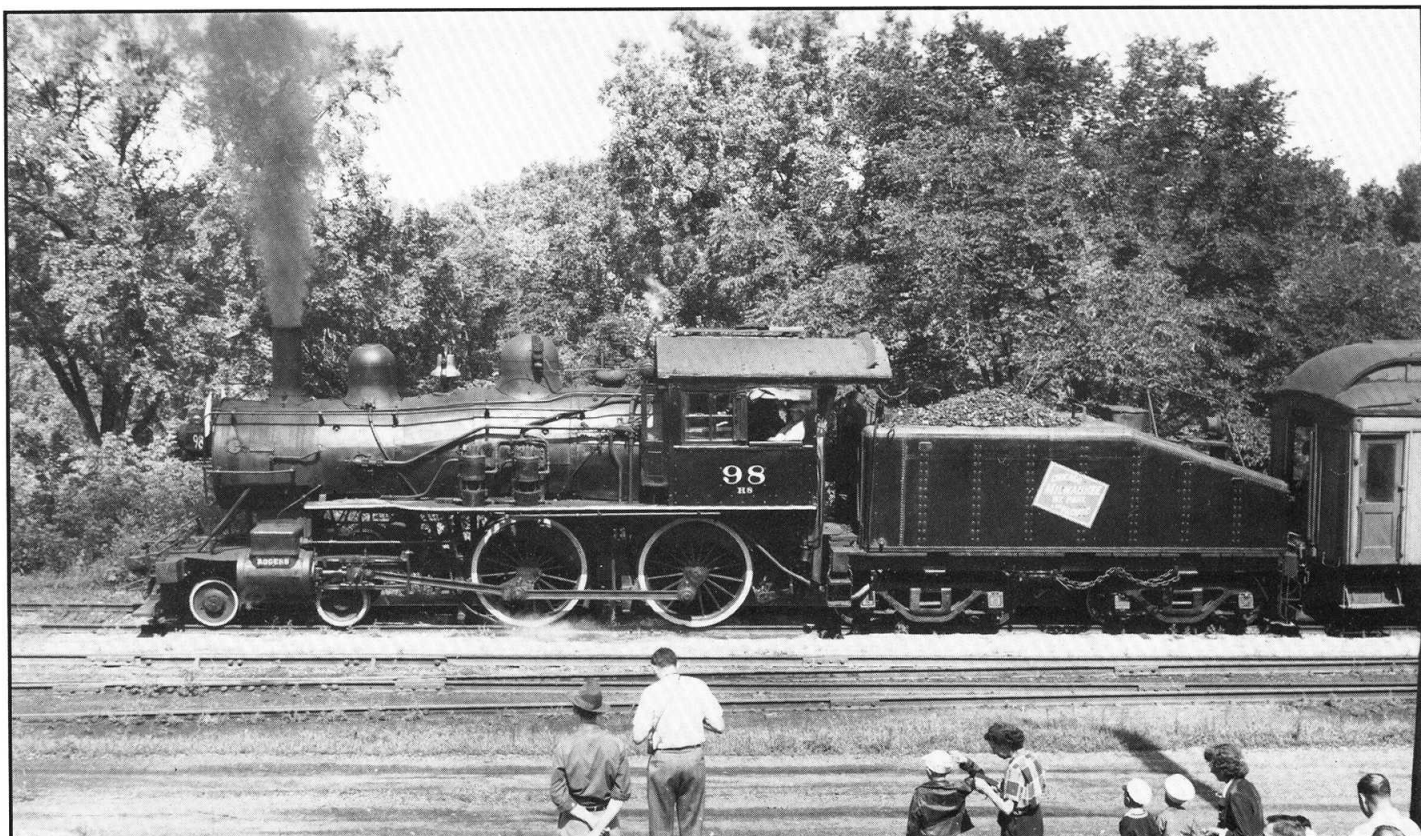
RN: We had enough to pay the bill and a little bit to boot. I would guess that our six cars were probably 80 to 90% loaded.

MG: Were they mostly railfans or did the general public show up?

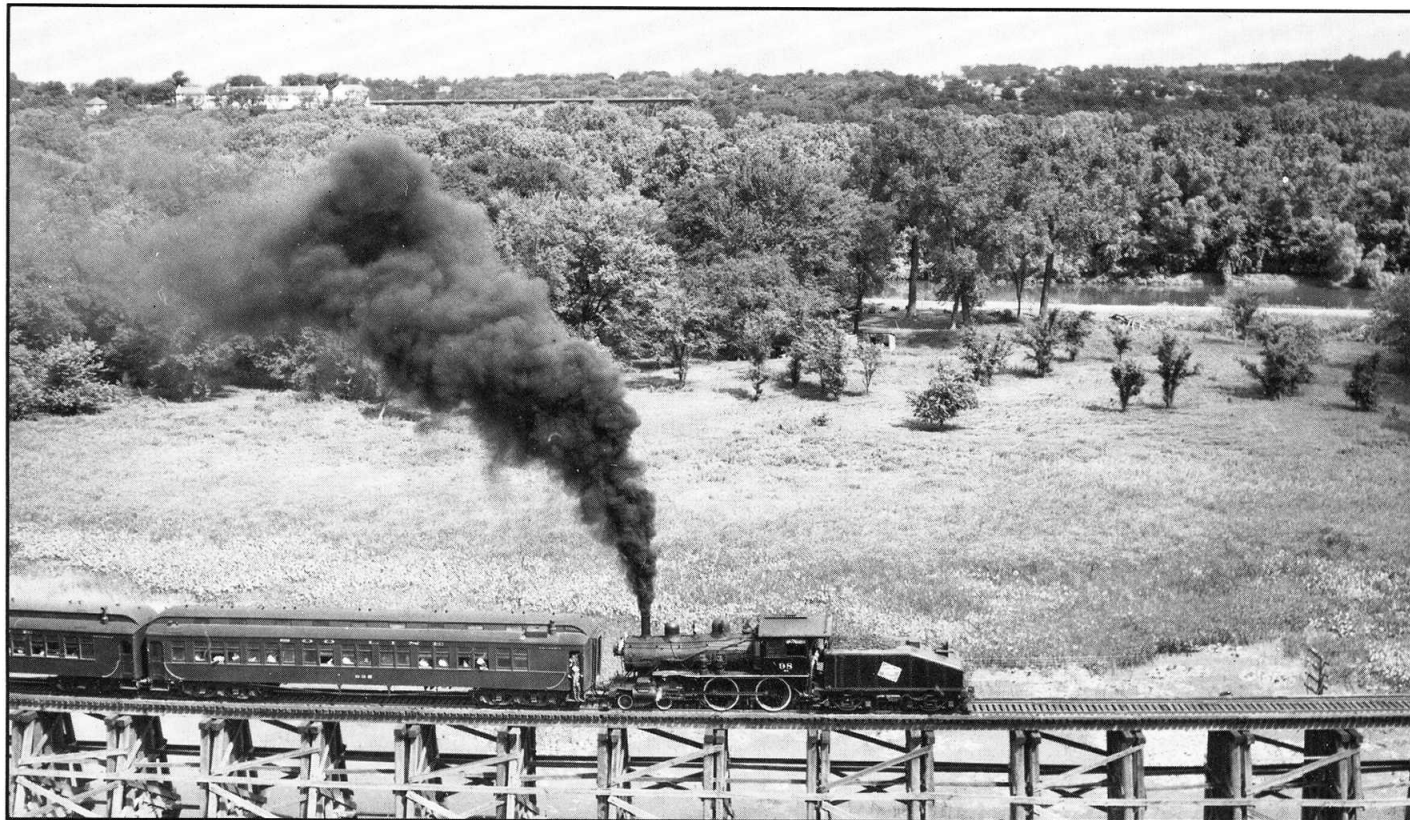
RN: We had quite a few general public, because we didn't have that many railfans around.

MG: After that trip, what came next?

RN: Well, the next one was a Milwaukee trip. That was kind of an interesting one too, because that



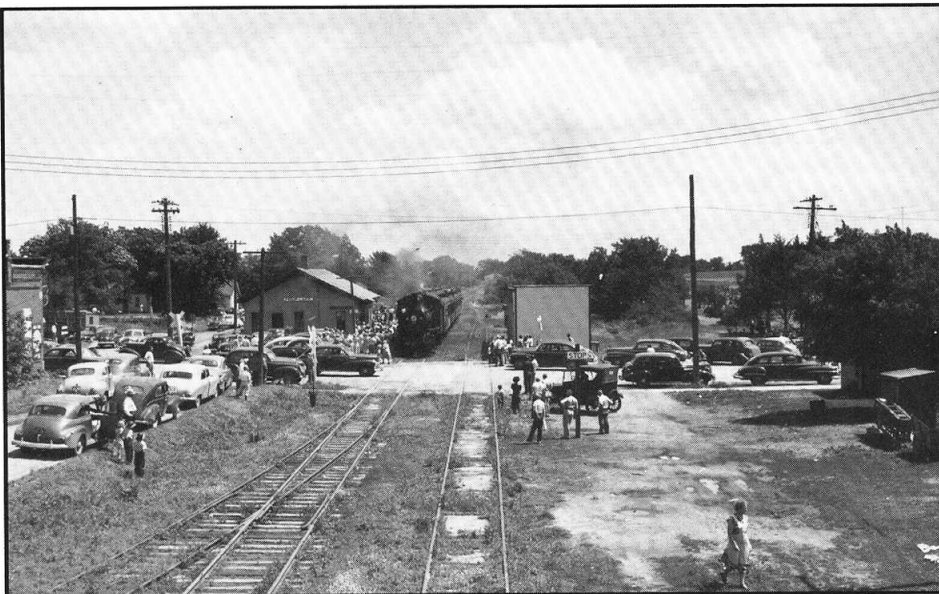
Note the builder's name on the cylinder. The tender came from a switch engine. R.V.Nixon photo. Ray Norton collection.



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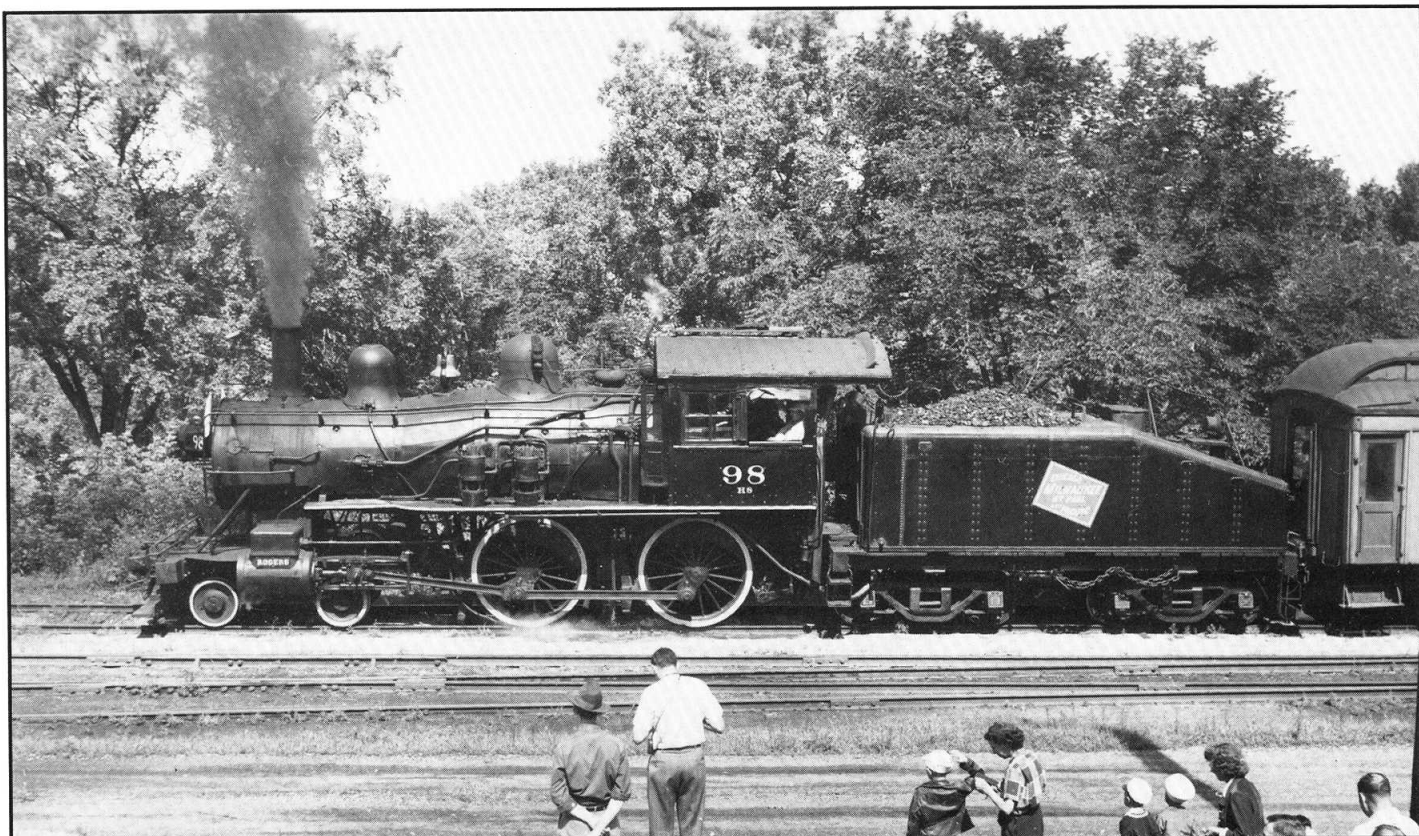
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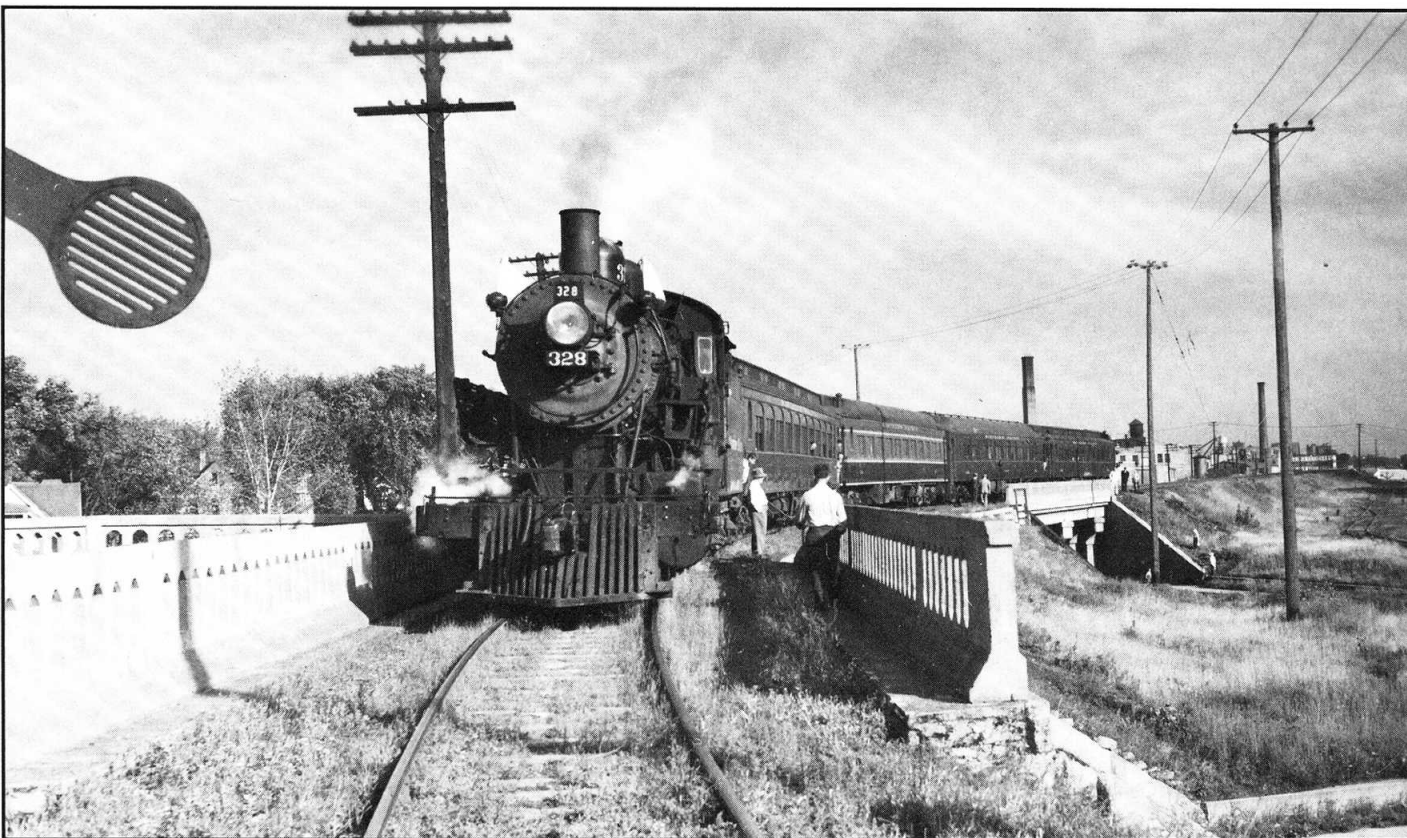


Note the builder's name on the cylinder. The tender came from a switch engine. R.V.Nixon photo. Ray Norton collection.



The Summer 1989 Minnegazette ran Marv Mahre's shot of #328 at Oak Glen before there was an Oak Glen, on the Sept. 18, 1949 fantrip. Here is the same location from on board the train. That may be Marv in the distance. John Malven photo. Ray Norton collection.

#328 has just left the NP "B" line at 18th Ave. NE and is waiting to cross the GN main. It will use the Mulberry Line on its way to the Mississippi River bridge. R.V.Nixon photo. Ray Norton collection.





The MRA special to Hutchinson leaves the Great Northern depot in Minneapolis. Ray Norton collection.

was one Charlie had tried to arrange and they didn't think the railfans could swing it so they said no, and then very shortly after the publicity we got on the Omaha trip, the Milwaukee came to us. I figured the circle tour out for them. We started out from the Milwaukee Depot tank first, with the old 98, which was a Rogers 1904 American standard locomotive. We had the three coaches from Soo Line, which were wooden stock drovers cars and then we had the combination baggage coach from the Hutch branch of the Milwaukee. We backed to Mendota. At Mendota we ran around the train. So we started out there locomotive, tender, combination car and coaches—a real legitimate passenger train. We went down to Farmington, and backed east down the old H&D main, then we went west to Cologne on the Hastings & Dakota, the old original part of the Milwaukee Road in Minnesota. And at Cologne, because there wasn't room on the wye, we just wye'd the engine and we came in with the combine bringing up the markers. That was another trip that was real close to on time and it was a beautiful trip. And how well I remember that old 98, cracking up that hill between Chaska and Carver.

MG: Tell us how the 98 happened to still be in service?

RN: The only thing I can figure is that it was a good little locomotive for light work and they frequently

used it on the Hutchinson out of Glencoe.

MG: I had heard that it was a shop switcher at South Minneapolis. Is that correct?

RN: Probably in its final days. The last mainline work it did was on that branch. It could have been that they were letting her boiler time run out as a shop switcher. Her tender sifter got to leaking so they put a tender off a switch engine and that detracted from its beautiful appearance.

MG: What sort of speed would that engine make?

RN: Faster than the track was good for. Except going down the main line and their main line from Mendota to Farmington was probably good for 55 miles per hour. It was a 67-inch driver engine and it would walk right along.

MG: How long did #98 last?

RN: It didn't last very long. I think that was close to her final days.

MG: What was next railfan trip that you recall riding?

RN: I don't know whether it was the

328 to Stillwater or the Milwaukee Road to Stillwater. On the 328 trip we went over to Mississippi Street roundhouse in St. Paul, down to 3rd Street, up the Scalley to White Bear and on down to Stillwater. They turned the 328 on the table there. Then we came on back up to M&D Junction and across the Minneapolis & Duluth through Roseville and East Minneapolis. At about 19th and Monroe NE we took the Mulberry line across by that malting outfit there, and went on back to downtown Minneapolis.

MG: What do you recall was the condition of the Stillwater line track when you rode it?

RN: I don't think it was as good as ours is today but it may have been right close to that. We didn't make any rash speed. It was kind of gentle. Same with the other trip when we went out there to dedicate the 328 in the park and we had the doubleheader, the T-1 Prairie type locomotive and the Pacific. The NP wouldn't let the two of them go into Stillwater because of that bridge over Highway 95. The Pacific took the train into Stillwater and it was too long to turn on the table. He came around the train and went tank first back up the hill. But he stalled just west of the Hazel Street crossing. So the Prairie backed down and the two engines were facing in opposite directions. That's the way we got up over Summit and into

Chicago, St. Paul, Minneapolis & Omaha

RAILWAY FAN TRIP

Sponsored by the

Minneapolis Railfans

Association

Good for one full fare

IN ADVANCE

\$2.49 N° 10299

AT TRAIN

\$3.00

A ticket on the first MRA fantrip cost \$3.00. Ray Norton collection.

Soo Line #2719 prepares for one of its last runs, an MRA excursion in 1959. Ray Norton collection.



White Bear. At M&D Junction they turned the Pacific and had a legitimate double header back to Minneapolis.

MG: What about the Milwaukee Road trip to Stillwater?

RN: That was a fun trip. We took off from the Milwaukee Depot and we went out through Mendota and then through the St. Paul Union Depot. At Hastings they used to have a long wye in between the H&D and the River Division. We pulled up the H&D and backed towards the River Division and then we were turned around headed back across the Mississippi River. When we got to the east side of the river we swung off and took the branch line up the St. Croix through Lakeland. It was just a beautiful trip. We got to Stillwater and ran the engine around the train and came on

back. One nice thing about both Stillwater trips is we had a couple of flat cars equipped with railings and we would ride out there so we could really get our fair share of cinders and sound. Our power was a ten-wheeler.

MG: There was a trip over the Great Northern to Hutchinson. Can you tell us about that?

RN: We had the 1724. It was an H-6 Pacific that was built out of an old J-1 Prairie. They stretched her a little bit and they put a Delta trailer on it and for awhile it had a booster. So I looked at the cab, and you know the Great Northern always painted the cab with the weight on drivers so that the crew would know what pay rate they were gonna get. Well, they had removed the booster on this engine, but they had not removed the paint job. So I asked the Road Foreman of Engines who was riding with us, "Are you gonna pay

that guy on the paint job on the engine or on the weight on drivers?" He said, "Weight on drivers". Then he looked at the engine and he saw that the weight of the booster was included and said, "We'll have to pay him the paint job until we get it changed." We had an auxiliary tender so we had 20,000 gallons of water and made the round trip on one fill of water.

MG: What sort of shape was the Hutch line in?

RN: The rail was real light but it was in pretty good shape. We monkeyed along at probably 20-30 miles an hour. They still had the wye for turning the engine.

MG: The Soo Line ran a couple of late steam trips in about '58 or '59. Can you tell us about those?

RN: The Soo Line had 2718, and H-27 type locomotive with 75-inch

drivers and it was capable of cruising right along. On one of the trips I remember we went close 70 miles an hour with a ten car train over 55 mile an hour track. That was because we had the Travelling Engineer running her. We took water at Dresser. There was a guy in Wisconsin that used to make tape recordings. He was aboard and made a recording of us leaving Dresser. That engine was so beautifully square. Those engines had a beautiful sharp crack of an exhaust. They were famous for it. And of course the reason for it is that the Soo Line did not hesitate to spend money on valve work. All of their locomotives were good and square. What I mean is, none of that chuck-a-lucka stuff, but it's bang-bang-bang, each exhaust just like the one ahead of it.

OVERHEARD AT THE CARBARN

The Editor and the Chairman, as is their custom, took a walk from the Editor's house to nearby Lake Harriet. This being a Saturday morning, it was also their custom to stop by the trolley. They passed Mike Buck, mowing and trimming the grass south of 42nd Street. The Chairman commented on what a fine operation this is. Even the grass gets mowed regularly.

They approached the carbarn. Car 1300 was waiting, pole up and headlight shining, having just returned to service after a major face lift. Once again, the Chairman exclaimed what a pleasure it is to have a real trolley line to manage.

Inside the barn, Bob Schumacher was talking with George Isaacs, who had a trolley pole harp mounted in the vise where he could work his will on it. The harp was the topic of discussion. It was uncommon, being one of three from the Minneapolis Anoka & Cuyuna Range recently donated by Harry Lee of Wichita, KS.

It seems that Mr. Lee had grown up in Minneapolis, riding the Oak-Harriet or Bryant to 54th & Penn cars from his home at 50th and Logan. As a student, he had commuted by trolley to the U of M, including the

Intercampus Special to the farm campus. Upon graduation, he moved to Columbus, OH and became active in the local trolley museum.

They were looking for any sort of useful streetcar equipment, and Lee heard that Northern Ordinance's truncated MA&CR had some to spare. Among the items he obtained were three trolley harps with wheels. Only one had been used recently and it was painted red.

Twin City Lines' eight-inch trolley wheels were probably the biggest in use anywhere. Many streetcar companies preferred sliding shoes instead. This was the case in Ohio, so the MA&CR wheels went unused.

When Lee moved to Wichita he carried them along. A group there is working to restore a Birney to operation, but once again, big trolley wheels were not in demand. Recently he chanced to travel back to Minneapolis and decided to donate them to MTM, 42 years after they first left town.

Now a donated object is not always a useable object. The harps and trolley wheels were bent and battered. George, never one to accept mechanical imperfection, had decided to restore them. And that is why the harp sat firmly in the vise, awaiting various applications of heat and pressure.

"Now I understand", said Bob, looking at the deformed harp, "why we dewired 21 times in 17 miles on the way to Anoka."

"Wait a minute", said the Editor. "You rode the Anoka & Cuyuna Range?" Service to Anoka had ended in 1939 and he hadn't expected an eyewitness to be this handy.

"Sure", Bob replied. "We'd catch it at the Interurban depot at 7th Street and 3rd Avenue North. The car would travel 1st Avenue through downtown, then cross the Hennepin Avenue bridge and take the 2nd Street line to its own track at 30th Avenue NE."

"And it lost the pole on TCRT wire?"

"Yeah, several times. It's just like with a model. The wheel has to be straight on the wire or it comes off. I remember that trip because a tornado had done a lot of damage in Anoka and we went up to take a look. Anyway, the track was just terrible. The car was making about 35 miles an

hour and we were bouncing all over the place. I was standing on the back platform and I could see the floorboards sliding back and forth. The seats actually moved from side to side. I don't think a steel car could have traveled that line. It wouldn't have been flexible enough.

They carried freight and express on the back platform and it was shifting around too much to suit the motorman. Those Anoka cars had no deadman on the brake, you know, so he just let the car run while he walked to the back and straightened up things. Suddenly he runs back to the front end. Seems that there was a big dip in both rails (Bob made a swooping motion with his hand) and he remembered it just in time to slow the car down. There were straight stretches where one rail was several inches higher than the other, while the curves might have no superelevation at all. Believe it or not, they had catenary overhead.

Anyway, when we got to Anoka, the wires were down on Main St., so we took the freight line part way around the north side of town and walked in from there. You know, you oughta talk to Bill Olsen. He rode the line more than I did."

As they started back to the Editor's house, the Chairman remarked that one of the benefits of museum membership is time travel, often when you least expect it.

THE PHANTOM STREETCAR

Editor's note: This item is reprinted from the July 24, 1953 issue of the American Transit Association's periodical, Passenger Transport, courtesy of John Diers.

The Twin City Rapid Transit Co. streetcar which is to make a run over the Chicago-Penn-Fremont and Selby-Lake lines tracks each Thursday, but not for the purpose of carrying passengers, made its bow last week.

In granting the company authority to replace streetcars with buses on the lines, the City Council stipulated that

the company must continue to run one car a week over the tracks so the city could maintain a continuing control over problems related to the conversion. The company and the city are at odds over removal of the tracks and restoration of pavement.

Riding with Motorman Kenneth R. Mellin aboard car No. 1665 last week were Benson (Barney) Larrick, vice president and general manager of TCRT; Luther Bakken, general superintendant of transportation; Hugo Erickson, city engineer; and Larry Fitzmaurice, a reporter.

The latter pounded out a feature story for "The Minneapolis Star" in which he said the group "had blazed a new fiscal trail over the abandoned Chicago-Penn-Fremont tracks." He added that at the end of the trip Mr. Erickson was able to report to the city that the streetcar company was properly maintaining its franchise.

A FEW RULES TO WORK BY.

Here are some scattered excerpts from the Dan Patch Electric Lines 1914 rulebook. Management expected a lot in those days, and nothing was left to chance.

"To enter or remain in the service is an assurance of willingness to obey the rules."

"Employees, in accepting employment, assume its risk."

"In case of danger to the Company's property employees must unite to protect it."

"Watches that have been examined by a designated inspector must be used by conductors, motormen and switch foremen. The certificate in prescribed form must be renewed and filed with the Superintendant every three months."

"When persons who are evidently intoxicated, idiotic or insane are seen in vicinity of tracks or stations, they should be guarded from approaching trains, and, as soon as possible, be put in the custody of the police or other proper authorities."

"When casualty occurs on any train on which passengers are

carried, trainmen must give their undivided attention to the care and comfort of passengers, especially any who may be injured. Bedding and linen may be taken from the sleepers for this purpose, the conductor keeping a careful account of all supplies so taken and accounting for any not returned."

"When a corpse is found on the right of way, remove the body so as to give a clear passage for trains, take steps for its protection, telegraph coroner of county in which body is found, and report facts to the Superintendent."

"The ground under wooden bridges must be kept free of grass, brush or rubbish. Where water barrells are provided they must be kept full of salted water and a bucket or rough board box kept submerged in each barrell."

"Employees of the company must devote themselves exclusively to its service, attending during the prescribed hours of the day or night, and residing wherever required."

"Station agents must acquaint themselves with the business interests of the people among whom they are situated, use all proper means to secure traffic and report all local matters which may affect the interests of the Company."

"Operators must consider all messages confidential, neither permitting them to be read by any person except those to whom they are addressed, nor making their contents the subject of conversation or remark."

"Conductors must allow no beggars, gamblers, hotel solicitors, or other unauthorized persons, to practice their vocations or distribute advertising matter on trains."

"A passenger who refuses to pay his fare, or to conform to any lawful regulation of the carrier, may be ejected from the vehicle by the carrier. This must be done with as little violence as possible, and at a usual stopping place or near some dwelling house."

"When passing through dining cars during meal hours, or through private cars, train employees other than the conductor when lifting tickets, will remove their caps."

"After departing from a station the name of the next station at which the train stops for passengers will be announced in each coach. On

approaching each station, the conductor or brakeman must again pass through each coach, stop twice, and announce each time distinctly, with closed doors, the name of the station."

"Freight brakemen must be on top of the train descending grades, approaching terminals, and through localities where extra precaution is required, to assist in holding train, or to pass signals."

"When running at night motormen must have in their cabs, where it cannot be seen by passing trains, a lighted red lamp with torpedoes attached, ready for immediate use."

"On hand cars one man must face the rear, so lookout will be kept in both directions."

"Except when protected by flag, track men must not use track jacks inside of rails."

"Track men must promptly kill animals injured beyond recovery."

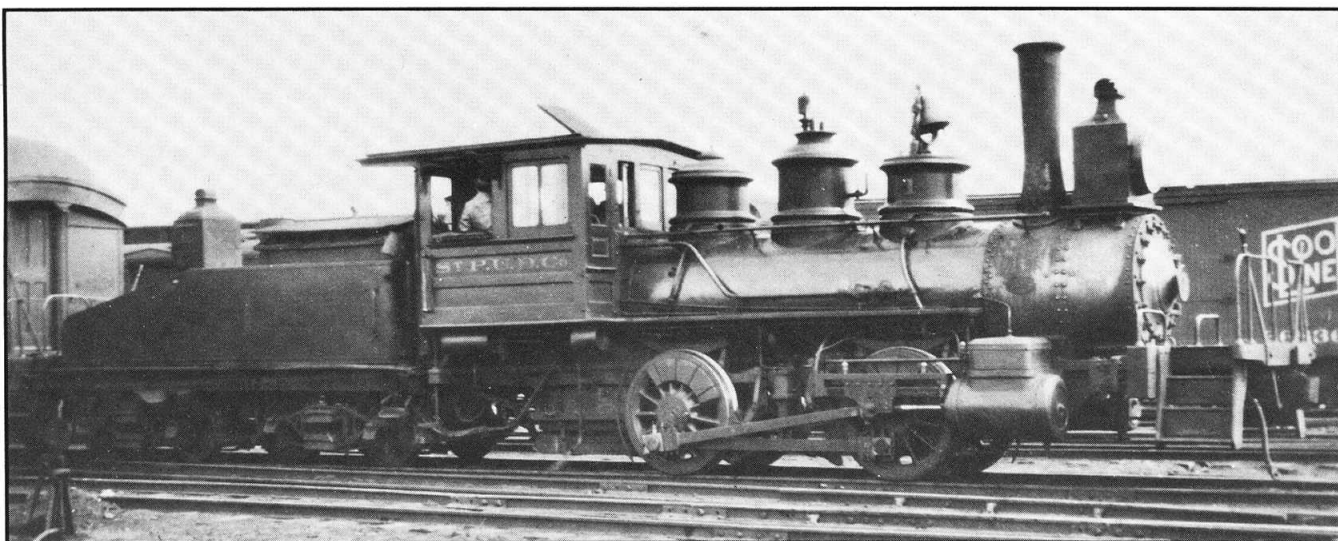
NO SPITTING

A TCRT Operating Department bulletin to motormen dated Aug. 24, 1916:

"Spitting through the front vestibule windows results in damage to window capping, wainscoting, bumper, etc. This not only means added expense, but causes complaint on the part of car cleaners, who seriously object to cleaning cars thus dirtied."

If it is necessary to spit, use the switch rod opening for that purpose, being careful that the floor of the vestibule is not splashed thereby.

It should be remembered that the act of spitting is often annoying and disgusting to observers. Motormen are urged to make every effort to avoid giving offense to passengers."



Above and Center: Here are two generations of St. Paul Union Depot Co. power. Management wasted no money painting multi-digit numbers on them. 0-4-0 #1 was built in 1883 by the Rhode Island works. Pete Bonesteel collection.

Below: The St. Paul Union Depot concourse is quiet these days, as the Upper Midwest Transportation Museum negotiates to buy it. Actually, this photo was taken in 1930 when the depot was new. Northwest Photographic Studios photo, Minnesota Historical Society collection.

TIMETABLE SAMPLER

Editor's note: This is a new regular Minnegazette feature. Readers are invited to submit interesting Minnesota timetables for publication.

In the course of a day's work, this Hudson, WI-based mixed train carried six different numbers, as it covered the Stillwater and Ellsworth branches. Only one connection was made with a mainline train—from the overnite Chicago-Minneapolis North Western Limited to Stillwater at 7:45 AM.

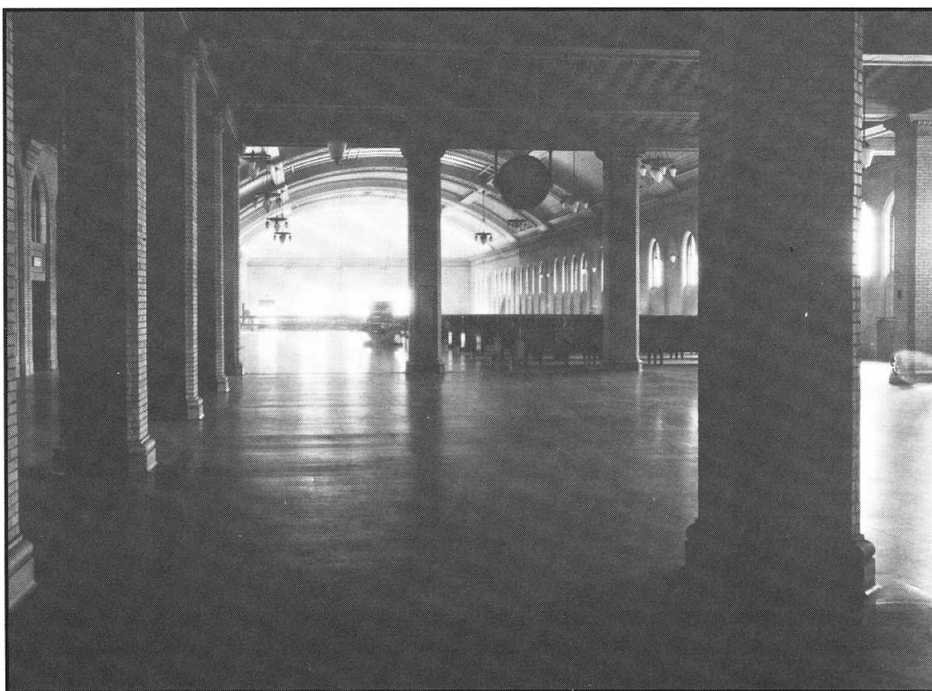
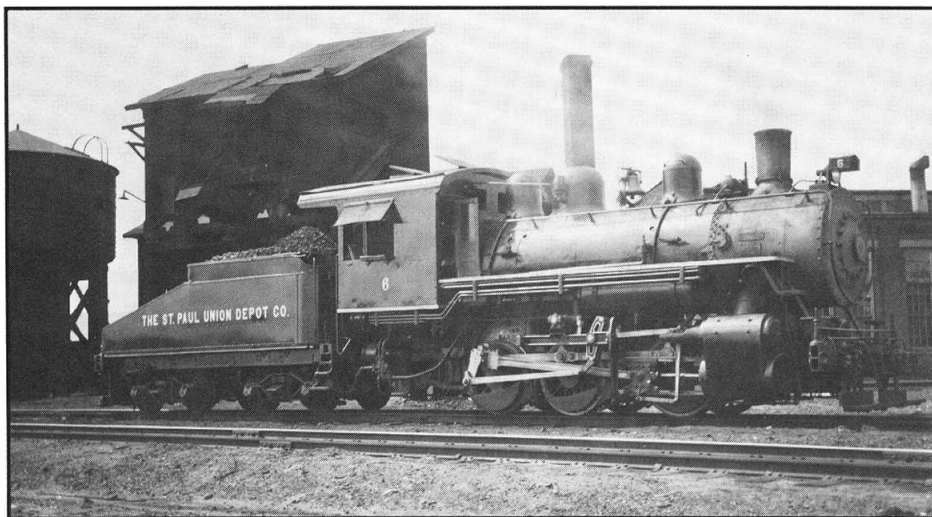
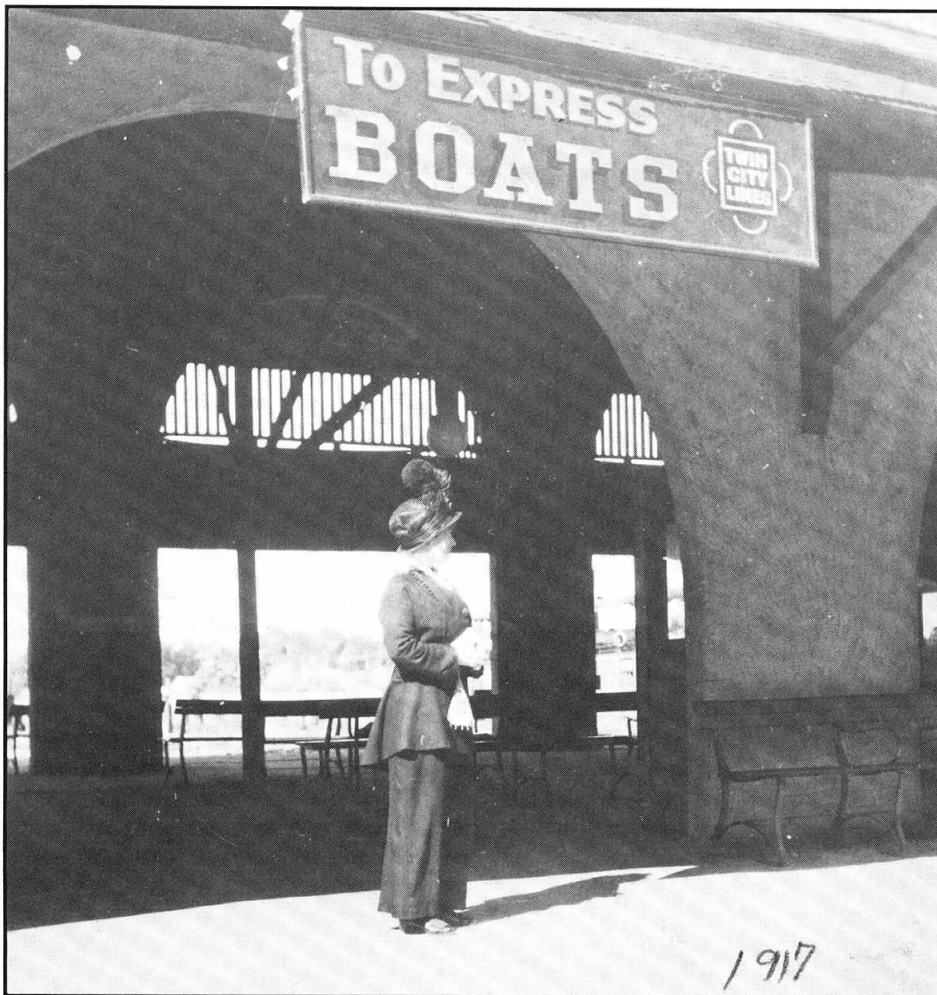


Table 23--STILLWATER LINE.

April 29, 1951.		Mls	601	627	.....
[LEAVE	.....	Mix.	Mix.	.....	.....
Hudson.....	+	0	74.5	11.40	.....
Bayport.....	+	3.4	8.00	1.53	.....
Stillwater.....	+	6.0	8.15	2.10	.....
[ARRIVE	.....	A	M	P	M
April 29, 1951.		626	616	.....	.....
[LEAVE	.....	Mix	Mix.	.....	.....
Stillwater.....	+	1845	230	.....	.....
Bayport.....	+	855	245	.....	.....
Hudson.....	+	915	300	.....	.....
[ARRIVE	.....	A	M	P	M

Table 24.
HUDSON-ELLSWORTH.

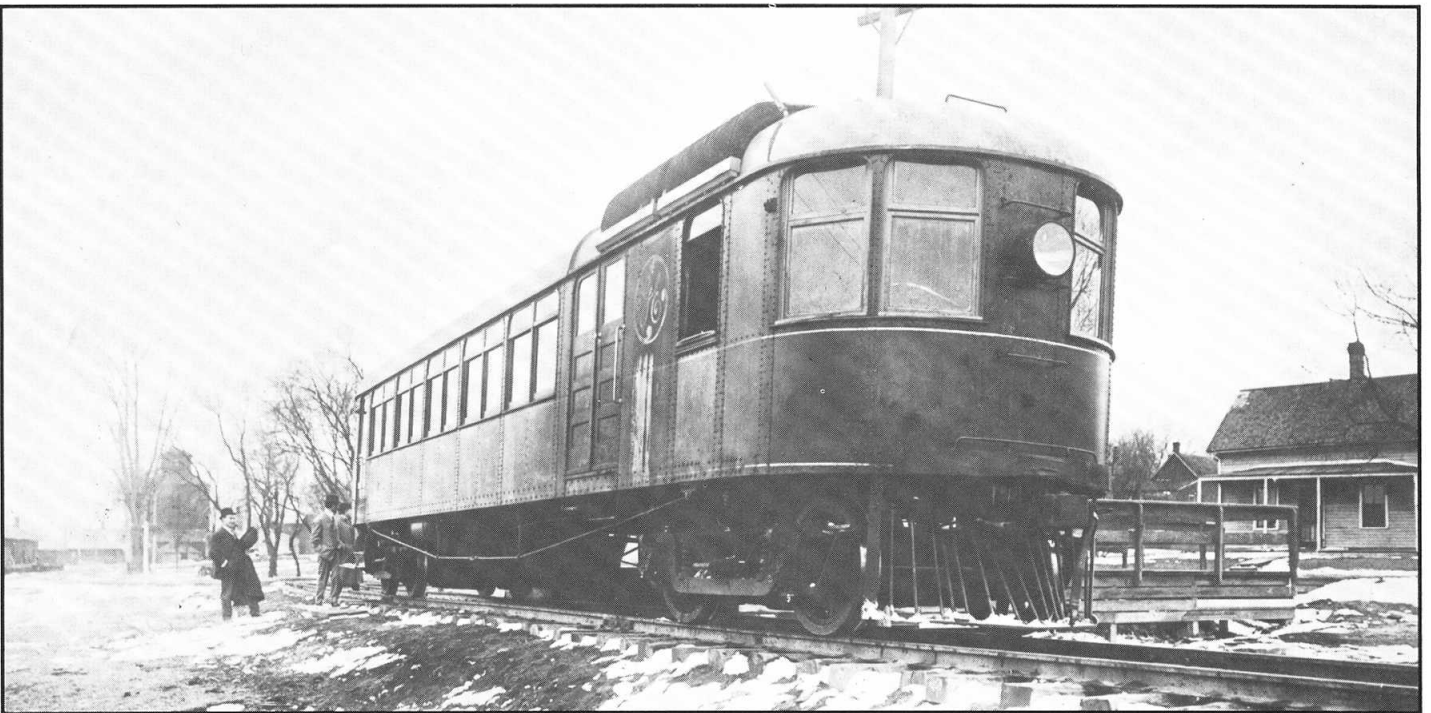
626	Mls.	April 29, 1951.		627
ΨAM	....	LEAVE	ARRIVE	ΨPM
†930	0	+....	Hudson.....	135
935	0.8	....	Hudson City....	127
1020	11.7	+....	River Falls.....	1255
1045	20.6	....	Beldenville.....	1206
1055	24.7	+....	Ellsworth.....	†155
A.M.	....	ARRIVE	LEAVE	A.M.



These three photos come from the family album of member Charles Eichhorn. Taken in 1917 and 1920, they show the Excelsior docks and the streetcar boats Como and White Bear.

Bottom Right: The first GE gas-electric demonstrator barnstorms on the Dan Patch Line, perhaps in Bloomington. Bloomington Historical Society collection.

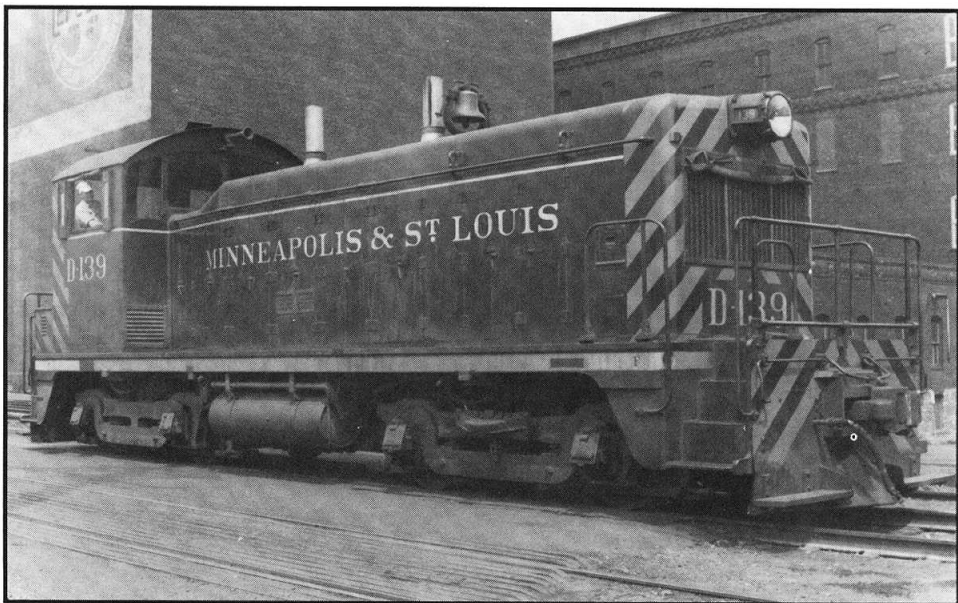






Above: Finishing the Mississippi, Hill City & Western's new depot in Hill City in 1910. This logging road was renamed the Hill City Ry. in 1915. It lasted until 1935. Here is the text from the back of the postcard, "Dear Sister Emma, This is our new depot. The train of 2 coaches leaves here at 7:10 AM and runs to Swan River and gets back at 2 PM the same day a distance of 23 miles, But they call it 25 miles and charge 3 cents a mile. They tie up here at the roundhouse."

Below: The first train of iron ore prepares to leave the Soo Line's Crosby depot on the Cuyuna Range. The depot is now a museum. The tracks are still there but unused. The last ore train ran in the 1970's. Both Pete Bonesteel collection.





Above: CMStP&O trains from Minneapolis to Omaha regularly met C&NW Chicago-Rapid City trains at Mankato. The Omaha train is at right. Pete Bonesteel collection.

Opposite Page Center: Dieselization on many railroads started with a few austere painted switchers. With a number like D-139, it was clear that this early NW2 wasn't considered normal power. It is switching on South 2nd Street in Minneapolis, next to the still extant Ceresota mill. Pete Bonesteel collection.

OBSCURE DEPOTS ON PARADE- Markville on the Great Northern Hutchinson branch was renamed Crystal Bay in 1906. A new Markville appeared on the Soo Line south of Duluth in 1911. The "Hutch" mixed stopped here until its demise, and the station site is evident to this day. Minnesota Historical Society collection.





Above: The short-lived Mesaba Railway in Chisholm in 1914.

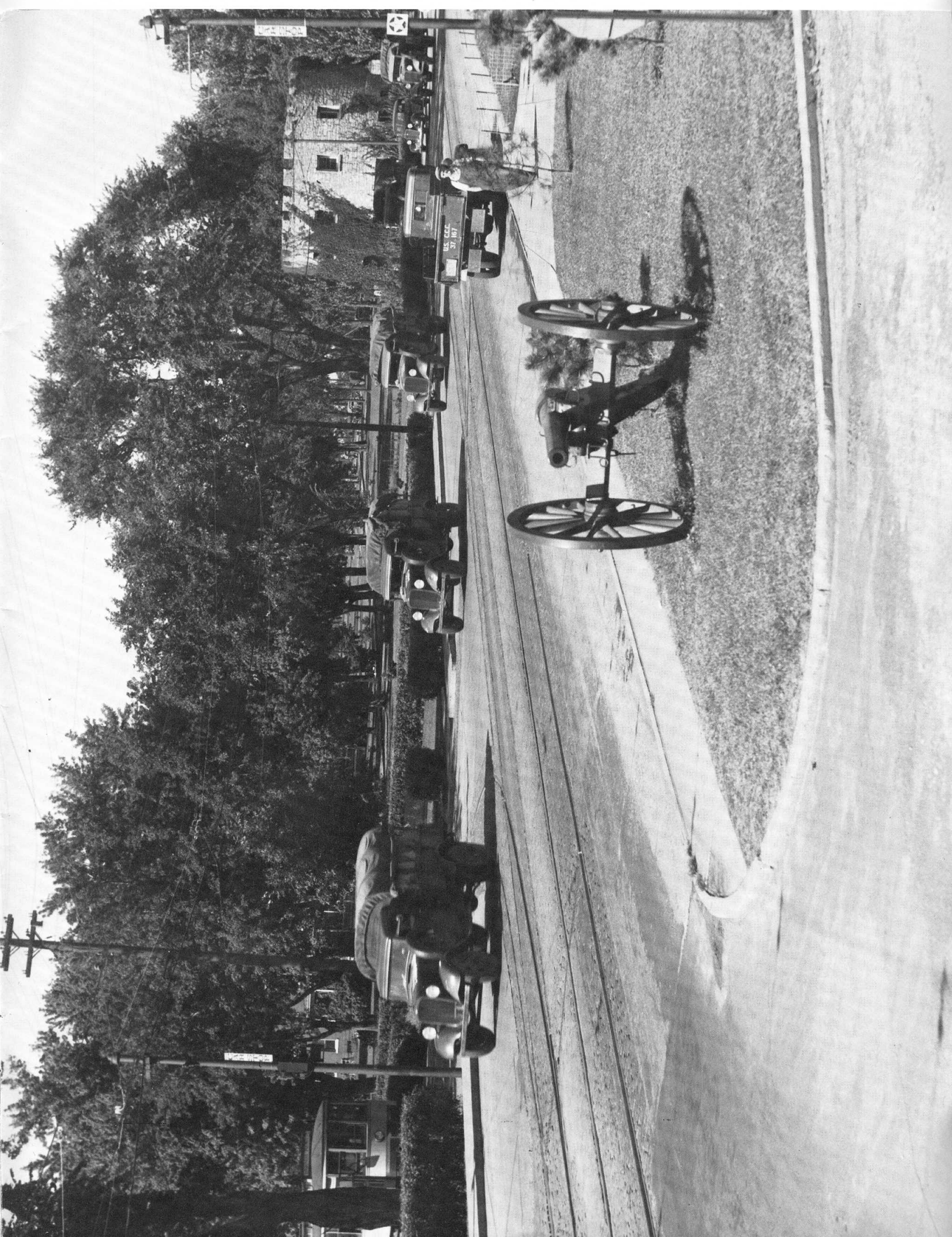
Below: The Omaha Road's St. Peter depot was across the river from downtown.

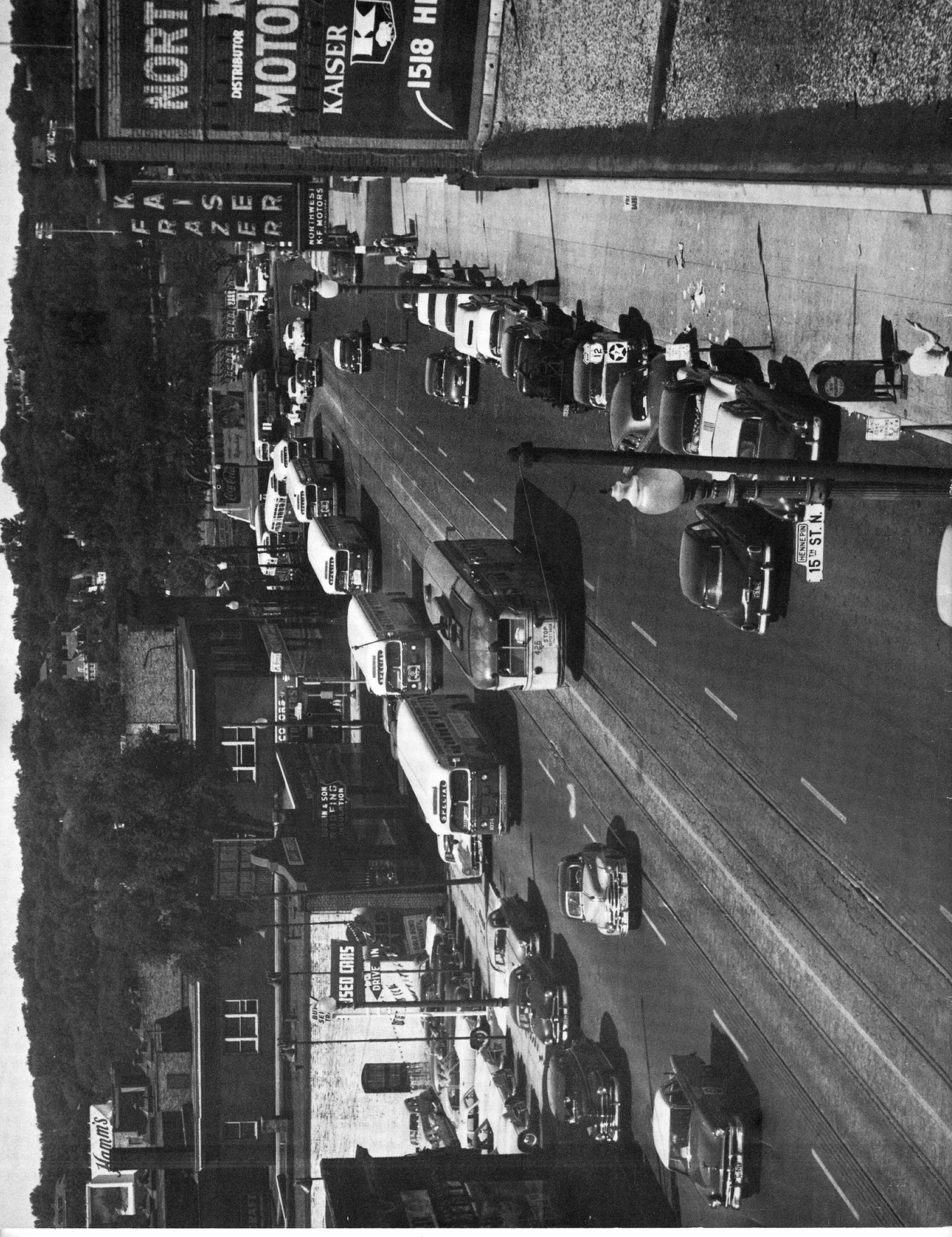
Both Pete Bonesteel collection.



Inside Rear Cover: The Editor frequents historical societies in the hope that pictures like this will turn up a crisp view of a non-rail subject with the train or trolley lurking in the background. Unlike so many railfan shots, photos of this sort put the trolley in the context of its surroundings. Consider this 1938 view of an army convoy leaving Fort Snelling. Note the pre-restoration Round Tower, the Civilian Conservation Corps truck and the archetypal working man with overalls, cloth cap, cigarette and hands firmly in pockets. All this quietly witnessed by car #1425 as it waits to return to Minneapolis. Minnesota Historical Society collection.

Rear Cover: It's July 8, 1953. The streetcars have a year left before the buses take over for good. PCC #426 confronts its replacements at 15th and Hennepin in Minneapolis. The buses are charters returning from Parade Stadium. Minnesota Historical Society collection.







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